

the great escape



From January 5th to 20th 2013
Lima - Tucumán - Santiago



CAR - TRUCK REGULATION



TOURING Y AUTOMOV.
CLUB DEL PERU



SUMMARY

The Specific Regulations, the Technical Regulations and the Assistance Regulations, as well as the Appendices must be considered as a whole and are therefore inseparable.

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SPECIFIC REGULATIONS



1P DEFINITIONS

1P1 ORGANISING COMMITTEE

Organisation Director:	Etienne LAVIGNE
Sporting Director:	David CASTERA
In charge of Logistics:	Marc PHILY
Director of External Affairs:	Grégory MURAC
Competition and Competitors' relations Coordinator:	Xavier GAVORY
President of A.S.A. of Paris Dakar:	Roger KALMANOVITZ

1P2 EVENT PROGRAMME

15 th May 2012	Opening of reception of the race application files.
15 th July 2012	Closure of the bike application files.
1 st November 2012	End of reception of the race application files and closure of entries.
22 nd & 23 rd Nov 2012	Boarding of the Vehicles in Le Havre (race and assistance Vehicles) and fixing of safety instruments (Iritrack, GPS - Sentinel, Tripy). Scrutineering for assistance Vehicles.
31 st Dec 2012, 1st & 2nd Jan 2013	From morning on 31st to evening on 2nd : Collection of Vehicles in Peru (Terminal Portuaria de Callao – Av. Contralmirante Raygada n°111 – Callao, provincial de Lima – Peru)
2 nd Jan 2.00-11.00pm, 3 rd & 4 th Jan 08.00 am to 11.00 pm	Administrative checks and technical scrutineering in Lima, then placing in Parc Ferme.
4 th January 2013	09.00pm: Race and Assistance Briefing in Lima
5 th January 2013	Podium (<i>the Podium time will be announced by a Bulletin</i>) Start of the 1 st Stage Lima-Pisco
13 th January 2013	Rest day in San Miguel de Tucuman (Argentina)
19 th January 2013	Last Stage La Serena-Santiago
20 th January 2013	Podium and prize giving in Plaza de la Moneda, in Santiago (<i>the Podium time will be announced by a Bulletin</i>)

1P3 POSTING

All information, official classifications, decisions, Bulletins and road opening notes and official documents will be posted each day at the following locations:

- In the waiting room in the administrative checks hall in Lima, on 2nd, 3rd and 4th January; in the Briefing hall and at the entry of the Parc Ferme in Lima, on 5th January 2013.
- In Peru, Argentina and Chile from 5th to 19th January 2013, at the PCO, in the catering area at the Bivouac and at the Malles Moto.
- At La Plaza de la Moneda, in Santiago on 19th & 20th January 2013, at the entry of the Parc Ferme and at the Competitors' tent.

The list of Competitors allowed to start will be posted in the Briefing hall, on 4th January 2013 at 09.00pm.

1P4 COMPETITORS' DEPARTMENT

From Monday to Friday, from 09.00am to 12.30pm and from 1.30pm to 6.00pm.

A.S.O. – Competitors' Department – Moto Club of Paris Dakar
Xavier GAVORY, Competitor Relations Service Coordinator
Nelly PAVÉE, Benoît LAVIGNE, Claire THOMAS, Chloé GIROU

Immeuble Panorama B – 253 Quai de la bataille de Stalingrad
92137 ISSY LES MOULINEAUX cedex
Tel. : +33 (0)1 41 33 14 60 - Fax : +33 (0)1 41 33 14 69
E-mail : concurrents@dakar.com

2P REGISTRATION

The Paris Dakar Sporting Association is organising, in conjunction with Amaury Sport Organisation (A.S.O. here after named the Organiser), the 35th Marathon Cross-Country Rally, named for 2013: « **Dakar Perú – Argentina - Chile** », an international event to be held from 2nd January 2013 to 20th January 2013, under the aegis of the Federation Internationale de l'Automobile, of the Fédération Française du Sport Automobile and of the National Sporting Authorities (ASN) of the countries to be crossed: Peru (TACP), Argentina (ACA) and Chile (FADECH).

Only the French version of these current Specific Regulations will be applicable.

In the case of legal litigation, France will be the sole country where cases may be judged before a tribunal and French law the only one applicable.

3P TERMINOLOGY

1) ASN

National Sporting Authority.

2) Assistance

Service shall be defined as unrestricted work on a competing Vehicle, or one of its elements, even when dismantled and/or the physical presence of a person(s) in the vicinity of a competing Vehicle.

3) A.S.O. Technical Passport (truck)

Document delivered by the Organiser authenticating a race Vehicle based on the Event technical and sporting criterias. This passport will be controlled during the Rally technical scrutineering by the Technical Stewards.

4) Bivouac

a) Zone situated between the Time Controls at the Finish of one Stage and the Start of the next, where all Competitors regroup; this zone is located in the Road Book. In the Bivouac, servicing is free between the Competitors still in the race and with Vehicles and/or people registered in the assistance category. It is a closed area, checked and secured, reserved for private use during the Dakar, whose access is exclusively and uniquely for persons who have been accredited by the Organisation, as well as representatives of local public authorities.

b) After having checked in at the Finish Time Control of the Stage, Competitors or Team members may take race Vehicles out of the Bivouac for refuelling, cleaning, to go to a hotel or for testing, within a radius of 30 kilometres of the Bivouac.

During these tests, the Iritrack must be switched on, on pain of penalties to be decided upon by the College of Sporting Stewards. All tests must be done outside of the route of Selective Sections.

c) It is forbidden to circulate at excessive speeds and/or to drive dangerously in the Bivouac area, on pain of penalties up to and including Disqualification from the race, to be decided upon by the College of Sporting Stewards. Speed is limited to 20 kph within the Bivouac.

d) Most important teams will be placed in the Bivouac. They will have to follow the rules and instructions given by the Organiser's staff.

e) It is strictly forbidden for the Dakar Competitors to display, outside the allocated spaces within the Bivouac, any kind of visibility support. It is also strictly prohibited to distribute any kind of product or merchandising (neither for sale nor for free) inside the Bivouac.

For any other promotional activity, the Competitors will, first, have to obtain beforehand the Organiser's written approval.



5) Briefing

a) The Briefing will be given by the Organisers' delegate. The Clerk of the Race will attend the Briefing.

Will be detailed the informations regarding safety and the route (amendments to the Road Book). These informations will be dated and signed by the Clerk of the Race, they will be posted on the official notice board, before or at the latest during the Briefing.

b) A general Briefing will be held on 4th January 2013, at 09.00pm in Lima.

Will be detailed the informations regarding safety and the route (amendments to the Road Book). These informations will be dated and signed by the Clerk of the Race, they will be posted on the official notice board, before or at the latest during the Briefing.

The presence of at least one member of each race and assistance crew is compulsory (signature) on pain of a penalty of €500.

c) **During the Rally, a Briefing will be held each evening at the Bivouac at 08.00pm, in the Dakar Information Center.** The presence of at least one member of the Crew is strongly recommended. **On the eve of border crossings and on the rest day, the Briefing will be held at 07.00pm.** After the Briefing, a summary will be posted on the official boards. It will give important information (Road Book's amendments) and the GPS code of the next Stage.

6) Bulletin

The official Bulletin is an integral part of the Regulations and is intended to modify, clarify or complete it.

7) CH (TC)

Time Control (see Article 36P). If, the Finish Time Control and the Start Time Control of the next Stage are combined, they must not be further than 5 km from the centre of the Bivouac for the Finish of a Selective Section and not further than 10 km from the centre of the Bivouac for the Start the next day.

8) Change of the maximum time allowed

On the evening of a Stage, if a certain number of Competitors having started the Stage have not crossed the Finishing line of the day's Selective Section, or if weather conditions stop the race from being run under normal conditions, the day's maximum time allowed may be changed by the Clerk of the Race at the College of Sporting Stewards' discretion.

9) Parc Ferme

Area in which no assistance, preparation, presence nor intervention is possible, except in the case of Article 41P.

10) Competitor

Physical or legal entity used for the physical or legal person who has entered the Vehicle.

11) Crew

"Crew" means the first Driver together with any co-Driver(s). It is made up of a maximum of four persons (a maximum of 3 in T1 and T2, a minimum of 2 and a maximum of 4 in T4) who must hold an FIA Driver's and entrant's licence for the current year, valid for the Event.

12) Disqualification

Sanction decided by the College of Sporting Stewards, following a severe infringement.

13) Duration of an Event

The Event starts with the administrative checks and/or scrutineering (including, if applicable, checks on the spare parts of the Vehicle), includes the Rally and ends upon the expiry of one of the following time limits, whichever is the later:

- time limit for protests or appeals or at the end of any hearings by the Sporting Stewards;
- end of the administrative checks and post-event scrutineering carried out in accordance with the Code;
- end of the prize-giving.

14) DZ

The start of a Speed Control Zone and, when possible, marked by a precise reference marker and by a WPS or WPE in the Road Book. In case of discrepancy between the two, the Way Point will be binding.

15) Estimated time

Time estimated by the Organiser to cover a Selective Section.

16) Event

Is considered to be the Event, the Rally Dakar named for 2013: "Dakar Perú – Argentina – Chile".

17) FIA technical passport

Document delivered by the Competitor's National Sporting Authority and validated by the Rally technical delegate who identifies the presented Vehicle with marks which cannot, under any circumstances, be removed. It must be presented to Technical Stewards at any moment.

18) Fixed Penalty

a) A Fixed Penalty has been created to replace certain sanctions leading to Disqualification for failing to respect certain clauses of the Regulations.

It allows the penalised Competitor to continue the Event in normal competition conditions, still being sanctioned.

b) The Fixed Penalty is expressed in a time which is added to the penalties already incurred by the Competitor.

c) The Fixed Penalty may be of a different figure for each Selective Section, Road Section or Passage Control, calculated according to the profile and the difficulty of each of these.

d) The total time attributed to the Competitor affected by the Fixed Penalty, for each Stage, will be calculated by adding together the maximum time authorised for the Selective Section(s) or Road Section(s) not covered, increased by the Fixed Penalty of the day for the missing section(s), as well as for any missing Passage Controls.

19) FZ

The end of a Speed Control Zone, marked by a WPE.

20) GPS Point - Way Point (WPT)

A Way Point is a geographical point defined by coordinates of latitude and longitude. There are 4 types of Way Points: WPV, WPM, WPE, WPS.

Each Way Point noted on the Road Book is a compulsory passage point.

21) Information note

This is information given by the Organisers and/or the Clerk of the Race to the Crews who, after reading it, must confirm this by signature. This note will be made available to the Competitors as soon as possible. An information note cannot amend the Regulations.

22) Infringement (Speed Control Zone)

An infringement consists of one or more instances of speeding in one and the same Speed Control Zone, defined by a DZ and an FZ. A second infringement will come about when one or more instances of speeding have occurred inside a new Speed Control Zone.

In case of repeated offences during the Event, the third infringement (in 3 different zones) may result, for all Competitors, in penalties that may go as far as Disqualification, depending on the excess speed noted.

A Speed Control Zone composed of 2 successive sections having 2 different maximum speeds will be considered as a sole Speed Control Zone.

23) Maximum time allowed

Maximum time given to each Selective Section. Any Crew exceeding this time, without any tolerance, will receive a penalty ranging from the Fixed Penalty to Disqualification. At that moment the control is said to be "closed" for the concerned Competitor.



23) Neutralisation

Time during which the Crews are stopped by the Clerk of the Race.

24) Official Information Board

Board on which are displayed information, decisions, amendments and official classification.

25) Official Itinerary

This is represented by the passage through each Way Point featured in the Road Book (WPV, WPM, WPE, WPS, DZ, FZ, CP).

26) Officials

The Clerk of the Race, Assistant Clerk of the Race, President of the College of the Sporting Stewards, Technical Stewards and the Crews' relations Officers are considered as "Officials".

27) PC (Passage Control)

A control zone where the Time Card must be stamped by the Stewards and which must be a WPM, a WPE or a WPS.

28) PCO/PC Course (Race Control)

Race Control (sporting and safety) and management of interventions.
The PCO will be operational 24 hours a day.

29) Pilots

a) **A.S.O. Elite Pilot:** Pilot featuring on the A.S.O. Elite list established by the Organiser.

b) **Amateur Pilot:** All Drivers who do not feature on the A.S.O. list of Elite Drivers, established by the Organising Committee.

30) Pulse signal (Speed Control Zone)

Conforming to the permanent functioning mode of the GPS, a pulse signal is recorded in the GPS. A pulse signal is recorded every 150 metres over the entire Selective Section and every 500 metres on Road Sections, with the exception of Village Speed Zones, where it will remain 150 metres.

31) Real time

This is the time actually taken to cover the route of a Selective Section.

32) Regrouping (Parc Ferme conditions)

a) A halt scheduled by the Organiser to enable the theoretical times to be observed on the one hand and, on the other, to regroup the Crews still racing. The regrouping time may vary according to the Crews.

b) The new start will be given according to the order of arrival of Competitors at the entrance of the regrouping Time Control. The first ten Crews that arrive will start at a 2-minute interval.

33) Road Book

Each Crew shall receive a Road Book, size A5, comprising a maximum of 5 horizontal lines of distances, drawings and information, containing characteristic notes and/or the compulsory Way Points, which they must observe on pain of penalties which may go as far as Disqualification.

34) Road Section

Section of itinerary with a target time between two successive Time Controls.

35) Route

This is defined by the official Road Book of the Event, confirmed by the crew of the opening car. The route is divided into Stages consisting of one or more timed Selective Sections linked by Road Sections.

The dividing up of the Selective Sections must be such that the majority of competing Crews can cover most of the route in daylight. It is prohibited to give the Start of a Selective Section at night. The maximum length of the Selective Sections per Stage is set at 800 km.

36) Selective Section

Speed test in real time. Selective Sections may be run over a course exclusively reserved for the Competitors of the Event.

Starts of Selective Sections (DSS) are preceded by or twinned with a Time Control and followed by a Time Control after the Finish (ASS).

37) Sporting Penalty

A Sporting Penalty means a penalty imposed for: speeding, missing a PC, or Way Point, or unsporting conduct, or other violation committed on a Selective Section.

38) Stage

Each part of the Event that is separated from the next by a Bivouac stopping time of at least 6 hours.

After a driving time of between 12 and 20 hours, a halt of at least 6 hours is compulsory. A halt of at least 18 hours is compulsory after 10 Stages of the race, unless otherwise decided by the Sporting Stewards, upon proposal from the Clerk of the Race.

39) Target time

a) Each Road Section will be covered within a target time, which must be respected by the Competitors.

b) Any Crew arriving ahead or after the target time on the Road Section will incur a penalty given to the minute (Article 36P1.8).

340) Team and Team Manager

A Team is a Competitor other than a physical person entering one or several Vehicles.

A Team Manager is a person duly authorised in writing by the Competitor to represent a Team entered in the Rally, with a Competitor's licence issued in the name of this Team (see Article 31P4).

41) Time Card

Document intended for the stamps, in chronological order, of the different control points scheduled on the itinerary.

42) Twinning with a bike Event

The interval between the last bike and the first car must be at least 30 minutes as from the first Stage that includes a Selective Section superior to 20 km.

43) Vehicle

Is a Vehicle any car or truck registered in the Event, passing through the technical scrutineering, complying with the criteria detailed in the current Regulations and having on board, the Crew.

44) WPE (Eclipse Way Point)

A compulsory passage point memorised in the GPS and indicated in the Road Book, and the coordinates of which are not revealed to the Competitors.

A point towards which the GPS, with all its capacities displayed on its screen, directs the Competitor once the Way Point preceding this WPE has been validated, whatever the distance between the Way Point and the WPE. It works in the same way between several successive WPE.

The Starts of Selective Sections will be WPE.

To validate their passage at a WPE, Competitors must pass within 200 metres of it. If the WPE is a DZ or a FZ the radius of validation is then 90 metres.

45) WPM (Hidden Way Point)

A compulsory passage point memorised in the GPS and indicated in the Road Book, and the coordinates of which are not revealed to the Competitors.

The GPS directs the Competitor towards this point only once he has come within a 800 meter-radius of it.



To validate their passage at a WPM, Competitors must pass within 200 metres of it.

46) WPS (Safety Way Point)

A compulsory passage point, for safety reasons, memorised in the GPS and indicated in the Road Book, and the coordinates of which are not revealed to the Competitors. It can be linked to a danger 3 (!!!), crossings of roads, gaz pipelines, railways, etc.

The GPS directs the Competitor towards this point only once he has come within a 3km radius of it. To validate their passage at a WPS, Competitors must pass within 90 metres to it.

The Finish of Selective Sections will be WPS.

47) WPV (Visible Way Point)

A point which coordinates are given to the Competitors by the Road Book and memorised in the "GPS".

Towards a Visible Way Point, all available information is displayed on the screen of the "GPS".

4P REGULATIONS

4P1 GENERAL

1) The Event is disputed in conformity with:

- the International Sporting Code of the FIA (the Code) and its appendices;
- the FIA Appendix J : articles 281, 282, 283, 284 (T2), 285 (T1) , 286 (T3), 287 (T4) ;
- the current Specific Regulations and its appendices;
- the FFSA Regulations;
- the Score Regulations (OPEN): 2006-2010 Off-Road Rules and Regulations (for the parts mentioned in the Technical Regulations in appendix).

Any change will be announced by a Bulletin.

2) Any claims concerning this application or any case not provided for will be studied by the Sporting Stewards who alone has the power to decide.

4P2 LITIGATION

In case of litigation the Clerk of the Race and the College of Sporting Stewards may take into consideration television images, photographs and data downloaded from Iritracks, GPS or Sentinel.

5P REGULATIONS AND PUBLICATION CONDITIONS

Not relevant to Competitors.

6P AMENDMENTS TO THE REGULATIONS - BULLETINS

1) The provisions of the Regulations may only be amended according to Articles 66 and 141 of the Code.

2) Any amendment or any additional provision will be announced by dated and numbered Bulletins, signed:

- by the Organisers up to the day of scrutineering, and stamped by the FFSA;
- by the Sporting Stewards throughout the duration of the Event.

They will be an integral part of the Regulations.

3) Once the Event has started, any amendment to the Regulations must also be announced by a Bulletin.

4) These Bulletins will be posted at the PCO and on the official notice board(s). They will also be directly communicated to the Crews, who must acknowledge receipt by initialling and must be made available to Competitors as soon as possible.

5) These Bulletins are printed on yellow paper.

7P APPLICATION AND INTERPRETATION OF THE REGULATIONS

- 1) The Clerk of the Race is responsible for the application of the International Sporting Code, of the FFSA's Regulations, of the specific Regulations and its appendices during the running of the Rally.
- 2) The Clerk of the Race must inform the Sporting Stewards of any important incidents that have occurred requiring the application of the present Regulations, the International Sporting Code and the FFSA regulations.
- 3) Any protests lodged by a Competitor will be sent by the Clerk of the Race to the Sporting Stewards for deliberation and decision (Articles 151 and the following of the Code).
- 4) Similarly, any case not provided for in the Regulations will be studied by the Sporting Stewards who alone have the power to decide (Article 141 of the Code).
- 5) Anything not authorised by this Specific Regulations is forbidden.
- 6) In case of interpretation discrepancies, only the French version of the Regulations will be taken into account.

8P QUALIFICATION OF AN EVENT IN AN FIA CUP

Not relevant.

9P ELIGIBLE VEHICLES

9P1 GENERAL

- 1) The Event is open to Vehicles of a maximum gross weight of up to 3,500 kg for Groups T1 and T2, and greater than 3,500 kg for Group T4, in due possession of a registration certificate. These Vehicles must comply with the safety specifications laid down by the International Convention on Road Traffic, by the FIA Regulations, by the current Regulations and its appendices.
The Event is open to Score Vehicles as well (Technical Regulations Article 3P).
- 2) Series production Vehicles which are not or no longer homologated in Group T2 may be authorised to take part in Group T1 with a T2 safety and preparation level. Vehicles must fully respect Article 284 (Group T2).
- 3) **4-wheel drive Vehicle with central or rear engine**
If a 4-wheel drive Vehicle has a technical passport drawn up before 31/12/2005, clearly indicating the position of the engine, it will be accepted even if it has an engine situated to the rear of the middle of the wheelbase. No modification done after 31/12/2005 on the passport will be allowed.
- 4) For all technical information, please refer to the Technical Chapter of this current Specific Regulations and to appendix J of the International Sporting Code (articles 281, 282, 283, 284, 285, 286, 287).

9P2 ELIGIBLE VEHICLES

- | | |
|---------------------|---|
| 1) GROUP T1: | IMPROVED CROSS-COUNTRY VEHICLES |
| Group T1.1: | Petrol 4x4 improved Cross-Country Vehicles. |
| Group T1.2: | Diesel 4x4 improved Cross-Country Vehicles. |
| Group T1.3: | Petrol 2-wheel drive Vehicles. |
| Group T1.4: | Diesel 2-wheel drive Vehicles. |

2) GROUP T2: CROSS-COUNTRY SERIES PRODUCTION VEHICLES

Vehicles homologated in the Cross-Country Group. Vehicles must comply with the specific conditions for Group T2 Vehicles – Appendix 2 of the FIA General Prescriptions.



Group T2.1: Petrol Cross-Country series production Vehicles.
Group T2.2: Diesel Cross-Country series production Vehicles.

3) GROUP T3: IMPROVED CROSS-COUNTRY VEHICLES AND LIGHT VEHICLES
Group T3.1: Light Vehicles over 1,050 cm³ with an engine able to be homologated in Group N or T2;
Group T3.2: Light Vehicles over 1,050 cm³ with an atmospheric petrol engine.
Group T3.3: Light Vehicles equipped with a bike's engine. These engines must strictly come from production and come from a large batch produced.

4) GROUP OP: "OPEN"
(see details in the Technical Regulations appendix of the current Regulations).
The Organising Committee reserves the right to refuse any crew in this group.

Group OP.1: ALL VEHICLES CONFORMING TO SCORE REGULATIONS
A Vehicle complying with the "Score" regulation may be accepted, on condition that the Competitor provides evidence that his Vehicle is recognised by the "Score International" organisation. The Vehicle must bear a safety inspection seal according to article GT2 of the Regulations governed by this organisation.

Group OP.2: 4 WHEEL DRIVE VEHICLES MORE THAN 2.8 TONNES AND LESS THAN 2.20 METRES WIDE

5) CHALLENGE NRJ: "ALTERNATIVE ENERGY"

a) In the T1, T2 and T3 Groups, there will be a challenge for Cross-Country Vehicles powered by alternative energy (biodegradable fuel, hybrides engines, ethanol, gas, hydrogen, etc.). Vehicles will be accepted in this challenge upon receipt of a detailed file, presented before 1st October 2012.

b) These Vehicles must have sufficient range to cover the distance from the Bivouac to the Finish of the Selective Section, except for 100% electric Vehicles which may, upon request, ask for a change of battery at bike refuelling (refuelling point organised for bikes throughout the Event), at the Start and Finish of the Selective Section. For Vehicles using biodegradable fuels, the fuel used to travel from the Bivouac to the Start of the Selective Section, as well as the Selective Section itself, must be the fuel declared at technical scrutineering.

c) Checks may be carried out at the start of the Bivouac and/or the Selective Section, and/or at the Finish of the Selective Section. As a function of any infractions, penalties will be awarded at the discretion of the College of Sporting Stewards.

d) The Selection Committee reserves the right to refuse any Vehicle and/or Crew in this challenge.

6) GROUP T4: TRUCKS
Group T4.1: Series Production trucks.
Group T4.2: Modified trucks.
Group T4.3 : Series or Modified trucks, corresponding to the technical specifications of Groups T4.1 or T4.2 and carrying out 'rapid assistance' for one or several Crews entered in the race (see Article 23P2).

The Organising Committee reserves the right to refuse any crew in this category.

In the Trucks' classifications, there will be a Challenge « under 10 litres » and a Challenge « 6x6 ». Trucks must comply with Appendix J – article 287, as well as the homologation Regulations of T4 FIA Group (01/01/2008) or the A.S.O. homologation and subject to obtaining the A.S.O. passport.

They must not be over 4.00 m high (official height of the bridges, electrical lines, etc. in Peru, Argentina and Chile).

Trucks whose length is between 9 and 11 metres must pay a supplement of €1,000, those with a length of between 11 and 13 metres a supplement of €1,500. Please contact the Organisers concerning Vehicles over these lengths.

10P TYRES

See Article 1P9 in the current Technical Regulations.

11P DATA SYSTEM

Not relevant.

12P CREWS

12P1 GENERAL

- 1) Any Crew entered by a Competitor is eligible; all Crew members must hold an FIA international competitor's licence valid for the current year.
- 2) When the Competitor is a legal entity, or the Competitor is not part of the Crew, the first Driver named on the entry form will be held responsible, jointly and severally, for all the liabilities and obligations of the Competitor, throughout the whole Event.
- 3) The full Crew must be on board the Vehicle throughout the entire duration of the Event, with the exception of the cases provided for in the Regulations. **If one member withdraws, or if a third party is admitted on board (unless this is to transport an injured person), the Vehicle shall be disqualified from the Event.**
- 4) During a Stage, transportation by land, water or by air of at least one member of the Crew by the Organisers or by a third party will entail the Disqualification of the Crew concerned.
- 5) The wearing of FIA and/or Score (depending on the category in which the Vehicle is registered) homologated safety equipment (Appendix L, Chapter 3) is compulsory throughout the Selective Sections, on pain of immediate Disqualification of the Crew.
The wearing of an FIA-approved safety harness is compulsory throughout the Event.
Officials may carry out checks on the Crews' safety equipment and clothing at the Start of each Selective Section in the Event and at any other moment of the Event. In the event of non-conformity, the start will be refused.
- 6) Crews and Competitors enter the Dakar fully aware of the risks that such an Event entails. Any incident between Competitors could be investigated by the Officials and the Organisers. If it is proven that this incident is considered as a "Race Incident", the liability of any Competitor could be entailed.

12P2 TRUCK CREWS

A truck Crew will be composed of a Driver and a co-driver minimum, and of a Driver and 3 passengers maximum. They must all hold a 2013 FIA licence.

In the case of exceptional permission from the Organiser, a truck may be authorised to take the Rally start with only one Crew member. A request must be made to the Organiser before 1st October 2012.

In the case of a Crew composed of 3 or 4 members, only 2 Crew members must have the truck driving licence.

In the case of withdrawal of one member for a 3-member Crew or two members for a 4-member Crew, the Vehicle may be authorised to continue the race, upon autorisation from the College of Sporting Stewards, as a function of the circumstances.

13P1 LIST OF OFFICIALS

The list of Officials will be announced via a Bulletin.

All members of the Organisation holding an FFSA, FIA, ACA or FADECH official's licence will automatically be considered as Judges of Fact, with the exception of members of the College of Sporting Stewards.

13P2 CREWS' RELATIONS OFFICERS

1) The Crews' relations officers will be easily identifiable by means of a distinctive marking. They are linked to the sporting power.

They will be present:

- at scrutineering and administrative checks,
- at the Start and Finish of Stages.

2) **The mission of the Crews' relation's officers is:**

- to inform the Crews and ensure permanent dialogue with them;
- to give accurate answers to all questions asked,;
- to provide all information or additional clarifications in connection with the Technical and Sporting Regulations;
- to avoid forwarding questions to the Sporting Stewards which could be solved satisfactorily by a clear explanation, with the exception of protests (for example, to clarify disputes over times).

3) **People in charge of the Crew's Relations**

Will be announced via a Bulletin.

14P ENTRIES

14P1 GENERAL

1) Anybody wishing to take part in the Event must send the race application file, duly completed, to the Competitors Relations Department, along with the entry fees, and mentioning at least:

- the full name, nationality, address, licence number (Competitor and/or Driver) and driving licence number of each member of the Crew;
- the characteristics of the Vehicle, the group and class.

Competitors, first Drivers or co-drivers of a different nationality from that of the Organisers' National Sporting Authority must comply with Article 70 of the Code.

This application request will be made through the opening of a file on the website www.dakar.com

2) By the very fact of signing the "Candidate Declaration", the Competitor and all the Crew members submit themselves to the sporting jurisdictions specified in the Code and the Regulations only.

Crew members must sign this "Candidate Declaration" during the administrative checks of the Event.

3) Up to the moment a Crew presents itself for administrative checks, any change of one or several members may only be made with the approval of the Organising Committee.

After the start of administrative checks and technical scrutineering, the changing of one or several members of a Crew may only be authorised by the Sporting Stewards.

4) Up to the moment of technical scrutineering, the Competitor may freely replace the entered Vehicle with another one.

5) Should it turn out, at the time of pre-race scrutineering, that a Vehicle does not correspond in its presentation to the group in which it was entered, this Vehicle may, upon the proposal of the

Technical Stewards, be transferred by the Sporting Stewards to a different group or be refused definitively.

14P2 ENTRY REQUESTS

- 1) Admissible, upon invitation, all persons over 18 years of age.
- 2) The Organising Committee reserves the right to refuse the entry of a Driver, co-driver, or a Competitor (Article 74 of the International Sporting Code). The application request may only be validated if accompanied by the entry fees.
- 3) Entry fees, cancellation of entry and requests for refunds: see Appendix n°1.

14P3 START REFUSED

- 1) No Crews refused a start due to failure to pass administrative checks or scrutineering will be eligible for refunds.
- 2) For refunds for additional services please refer to the general conditions of sale laid down by the different suppliers.

14P4 CANCELLATION OR POSTPONEMENT OF THE EVENT

1) In the case where it proves impossible to run the Event, for whatever reasons, and particularly for the following non-exhaustive reasons such as: failure to obtain, or withdrawal of federation approval, failure to obtain authorisation to cross a particular country, political problems in one or other of the countries to be crossed, making it impossible to run the Event, financial difficulties making it technically or sportingly impossible to run the Event, embarkation, disembarkation or logistical problems making it impossible to transport Competitors, etc. The Organiser will be liable only for funds paid.

Funds received by the Organiser will be refunded by the 31 March 2013 at the latest.

2) In the case where the Event is delayed, the Organiser will inform each Competitor immediately, by registered post, of the new programme for the race.
In the case where participants are unable to take part in the race, due to the change of date, they have a maximum of eight (8) days, from the moment the registered letter is received, to request, by registered mail, refund of funds paid to the Organiser.

This refund will be paid by 31 March 2013 at the latest.

3) In all cases, participants may only claim a refund of funds paid.

15P IDENTIFICATION

1) The Organisers shall supply each Crew with a set of identification plates comprising: 2 "Rally" plates and 3 panels bearing the race numbers which are called number plates.

2) Throughout the duration of the Event, the plates must be affixed according to the present Regulations.
In no case may they cover, even partially, the Vehicle's licence plates.

3) The number plates (dimensions still being studying on) must be affixed to the right and left sides of the Vehicle, on the area situated between the wheel arches, provided that they are totally visible from the side, as well as on the roof of the Vehicle, legible from the back.
They bear the race number, the name of the Event and possibly the name of the Organisers' main sponsor. 26 cm x 50 cm are for compulsory Organiser's advertising.

4) 2 "Rally" plates must be positioned legibly in a visible position during the whole Event. They must be fixed at the front and the rear of the Vehicle, parallel the axis of the wheels, without covering,



even partially, the Vehicle's license plates and, except for the manufacturer's acronym, the plate must be the first inscription legible from the front above the line of the headlamps.

They incorporate the Competitor's race number in figures of 4 cm high and with a stroke thickness of 1 cm.

5) At any time during the Event, the absence or faulty positioning of a plate may incur, on certification, a cash penalty equivalent to 10% of the entry fees (Vehicle and Crew fee), on condition that the Organisers provide these plates. The absence or faulty positioning of 2 or more plates at the same time may result in a cash penalty equivalent to 20% of the entry fee (Vehicle and Crew fee).

6) The names of the first Driver and his co-driver(s), plus their national flags (nationality of the licence), of a height of 30 - 50 mm, must appear on both sides of the front wings or front doors of the Vehicle. Any Vehicle failing to comply with this rule may be subject to a cash penalty, on certification, equivalent to 10% of the entry fee (Vehicle and Crew fee).

7) An identity bracelet equipped with a RFID chip, on which will be printed **the phone number of the PCO (+33.1.41.33.15.81)**, will allow the identification of Crews. Crews and Competitors bracelets will be differentiated. Failure to wear this bracelet, noticed by an Official, will result in a penalty of 10% of the entry fee (individual fee). In the case where the bracelet becomes damaged the person must request a replacement from the Competitors' Department, in exchange for the damaged item.

16P ADVERTISING

16P1 GENERAL

1) Crews are allowed to affix any kind of advertising to their Vehicles, provided that:

- a) it is authorised by the FFSA and the FIA Regulations and the legislation of the countries crossed,
- b) it is not likely to give offence,
- c) it does not encroach upon the spaces defined below reserved for Rally plates, number plates and windscreen strips,
- d) it does not interfere with the crew's vision through the windows,
- e) it does not express either a religious and/or political opinion.

2) The places reserved for the Organisers for collective advertising which may not be bought are situated on:

- f) the number plates where the Organisers' advertising must be affixed on a strip measuring 26 cm x 50 cm, which may be divided above and/or below the numbers,
- g) one set of "Rally" plates, of which 9 cm x 43 cm is reserved for the Organisers' advertising,
- h) two strips to be affixed on each side of the upper part of the windscreen, 10 cm high by 25 cm long.

3) The Organisers' optional advertising will appear on two 50 cm wide x 52 cm high plates which cannot be subdivided, to be affixed on the right and left sides of the Vehicle, on the area situated between the wheel arches provided that they are totally visible from the side.

4) For Competitors who refuse the Organisers' optional advertising, the amount of the entry fees per person will be increased by 60%, i.e.: €17,600 including taxes individual fee and €21,280 (solo fee) for cars and trucks.

5) The Crews must ensure that the advertising is properly affixed throughout the entire Event from the moment they leave scrutineering. If compulsory or optional advertising is absent or wrongly fixed, a penalty of 10% of the entry fee (Vehicle and Crew fee) will be incurred for the first offence, and 100% of the entry fee for each repeated offence.

16P2 SPECIFICS

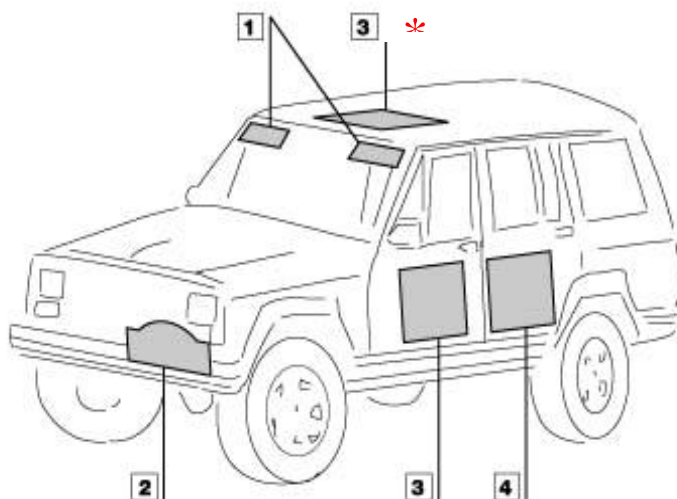
Competitors are required to provide sufficient space to conform to the above Regulations, with all modifications to stickers forbidden (cutting, etc.).

For the T3 buggy category, the Organisers' optional advertising, as well as the racing numbers, will appear on 31x30 cm plates, with the exception of the roof plate, which will be 50x50 cm.

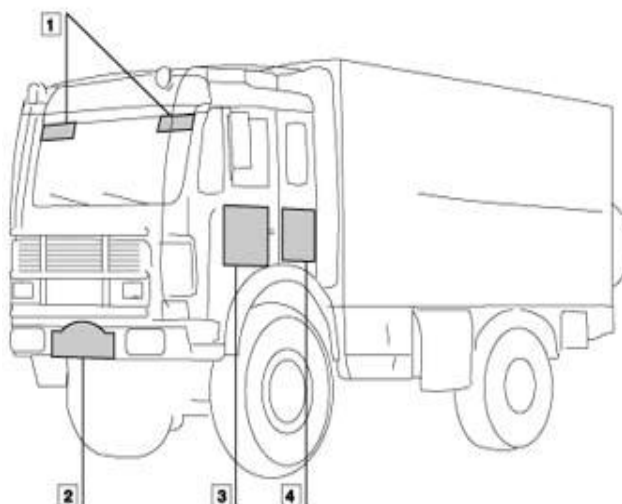
List of Organisers' advertisers = will be communicated on a Bulletin.

Optional advertising = will be communicated on a Bulletin.

1 – 4x4 Traditionals



2 – Trucks



- ① 25 cm of windscreen 'sun' visor strip at each extremity, by 10 cm high, compulsory.
- ② 2 Rally plates, 43 x 22 cm, at the front and rear of the Vehicle, compulsory.
- ③ 3 race number plates, 50 x 52 cm, compulsory, except for Buggys. That of the roof must be legible from behind.
- * The roof plate is not mandatory for assistance vehicles.
- ④ 3 race plates, 43 x 45 cm, mandatory, only for Buggys.
- ⑤ 2 Organisers advertising panels, 50 x 52 cm, optional.

16P3 COLOUR OF THE NUMBER PLATES AND ADVERTISING

So as to easily differentiate different groups, plates will be of different colours:

- **T1, Open, T3 and T4:** white plates;
- **T2 and T4.3:** pink plates;
- **Assistance Vehicles :** yellow plates.

17P MARKING

17P1 GENERAL

The engine block and chassis identified with a mark during scrutineering, and the Crew are associated with and identified by a race number; these three elements can be neither changed nor replaced during the running of the Event (except for cases expressly provided for in the present Regulations).

The GPS is personalised and marked by a seal, a series number is attributed to a Crew. No change can be made without authorisation from the GPS supplier. All switching of equipment between Vehicles is forbidden, on pain of penalties that may go up to and including Disqualification.

2) Any anomaly discovered, and in particular identification marks presented as original and untouched which have been tampered with, may entail the Disqualification of the Crew, as well as that of any other Competitor or Crew who has helped or been involved in the commission of the infringement. This will not prejudice any demands which may additionally be made to the Competitor's or accomplice's National Sporting Authority concerning the imposition of heavier sanctions.

17P2 ALL GROUPS

1) Before arriving at scrutineering, it will have to be provided by Crews , on the parts listed below, a hole allowing fixing of seals, on pain of being refused the start. The Competitor is responsible for the existence of all marks and seals during the Event.

The absence of a hole allowing the passing of a seal will incur a penalty of €150 by missing hole.

2) Engine Block

Ensure a hole allowing the passing of a seal (diameter = 3,5mm minimum).

3) Air Intake Restrictors

Ensure on their engines, a hole allowing the passing of a seal to mark the engine air intakes (diameter = 3,5mm minimum).

17P3 T1 GROUP

Suspension travel

Ensure a hole allowing the passing of a seal (diameter = 3,5mm minimum) on the bump stops. The hole must be placed in a way that it is not possible to adjust the suspension travel without breaking the seals.

18P ATTRIBUTION OF RACE NUMBER

The Organising Committee is the only entity apt to attribute race numbers.

Race numbers will be attributed on the basis of the following criteria:

- Drivers featuring on the A.S.O. list of Elite Drivers;
- results obtained on preceding Dakars and/or on Dakar Series and/or on World Cup for Cross-Country Rallies (except Bajas);

- sporting or media notoriety of a Driver and/or Team;
- category and class of Vehicle (T1, T2, T3, OPEN, 2 or 4-wheel drive, etc.).

19P DRIVERS' PRIORITIES

Any Driver registered on the A.S.O Elite Drivers List, shall be on it for the Dakar 2013. The list will be made every year, published in its temporary form in the current Regulations and published in its definitive form after the closing of entries.

20P STARTING ORDER

20P1 GENERAL

1) If two or more Selective Sections have been run in the same Stage, the starting order of the next day's Stage will be drawn up by cumulating the times of these Selective Sections. In case of a dead heat, priority will be given to the Crew who achieved the fastest time in the first Selective Section.

2) In the case of two or more successive Selective Sections, the start of the following Selective Section shall be based on the finish time of the previous Selective Section in hours, minutes and seconds, to which will be added the target time for the Road Section, if any.

3) The Clerk of the Race, when drawing up the starting order, will take into account any Sporting Penalties that a Competitor has incurred for infringements (missed PC, missed Way Point, speeding, unfair behaviour, etc.) committed during the preceding Selective Section, and which will be added to the time of the Selective Section(s) concerned.

This procedure is applicable provided that the penalties are noted down on the Competitor's Time Card and/or established by any other means at the disposal of the Race Direction.

4) Any penalties incurred on the Road Sections shall be added to the general classification of the Stage covered.

5) In a case where a Crew arrives early at the Time Control before a Start for a Selective Section, no modification to the arranged starting order is authorised as a function of the cars present, and in all cases, the starting time is the target check-in time at the Time Control + 5 minutes, even if it concerns the Start for a Selective Section other than the first of the day.

Example:

Target check-in time at Time Control: 10.00am; actual early arrival time: 9.54am; theoretical starting time: 9.59am; Actual starting time authorised = 10.05am, which corresponds to the target check-in time + 5 minutes.

The marshal in charge of the start must ensure that this rule is respected and make a report to the Clerk of the Race. Any infringement of this rule may lead to a sanction imposed by the Sporting Stewards, which may go as far as the Disqualification of the Crew concerned.

20P2 STARTING ORDER

1) Prologue

a) A prologue will be organised during the Stage n°1. The Vehicles will leave in the order of their race numbers. The gap between Vehicles will be specified via a Bulletin during administrative checks in Lima.

b) The prologue will define the starting order for Stage n°2. For cars and trucks, the winning Vehicle will start first, the second will start in the second place, and so on. The time done for the prologue will be counted in the general classification with a 5-ratio. A Vehicle achieving a time of 5 minutes and 30 seconds will be attributed, at the general classification, a time of 27 minutes and 30 seconds. The prologue total distance will be approximately 12 km.



c) In the case of a Vehicle withdrawing during the prologue, the Vehicle will be allowed to take the start of Stage n°2 being the last one to start, penalised by the maximum time plus the Fixed Penalty.

2) Starting Order

a) For the 2nd Stage, the cars will start in order of the prologue classification: the first 10 cars every 3 minutes, the following 10 every minute, the remainder every 30 seconds. The trucks will start mixed with the cars in the order of the combined car/truck prologue classification

b) From the 3rd Stage, the Vehicles will start in order of the previous day's Selective Section: the first 10 cars every 3 minutes, the following 10 every minute, the remainder every 30 seconds.

The T4.1 and T4.2 trucks will start in the order of the combined overall car/truck results of the Selective Section of the previous day, whenever possible.

When this is not possible, the T4.1 and T4.2 trucks will start behind the cars, the first 5 every 2 minutes, the 5 next every minute, and every 30 seconds for the remainder.

The T4.3 trucks will always start behind the cars/T4.1 and T4.2 trucks mixed, every 30 seconds, in the order of the result of the previous day's Selective Section.

3) Gaps

The gap between the last bike and the first car will be a minimum of 30 minutes and a maximum of 2 hours for a Selective Section superior to 20 km.

The gap between the last car and the first truck will be fixed at the end of the reconnaissance and will be given out at administrative checks.

4) Timing (itinerary/schedule)

The starting order and starting gap for each Stage will be specified on the timing (itinerary/schedule) given out during administrative checks.

The starting order and starting gap for the start podium (5th January 2013) and finish podium (20th January 2013) will be subject to an information note.

5) Posting of starting orders

On the evening of each Stage, the starting order of the first 20 will be posted at 7.00pm. Vehicles who have not arrived by 10.00pm will start after the others, in the order of their starting order of the previous day. The start list of all Vehicles will be posted at 11.00pm at the latest.

20P3 RECLASSIFICATION

For safety reasons, at the end of each Stage, the Sporting Stewards will automatically reposition for the starting order of the next day's Stage, the A.S.O. Elite Drivers and/or the first 20 overall in the car category, the first 10 overall in the truck category, if these Drivers happened to be badly classified due to mechanical problems. In no case can such a repositioned car start in front of a car appearing among the first 20 cars in the start list of the Stage. In no case can such a repositioned truck start in front of a truck appearing among the first 10 trucks in the start list of the Stage.

21P ROAD BOOK AND NAVIGATION - GENERAL

1) The route of the Rally is described in the Road Book which is given to the Crews. It is validated by the opening team during the verification of the route.

2) The Vehicles are obliged to be equipped with one or two GPS downloaded with the Way Points given by the Organiser. If the official GPS is broken, the second GPS becomes the official one. The spare GPS will be identified by the race number xxx of the Competitor added with a "S": xxxS (Spare).

3) Various compulsory passage points (Way Points) noted during reconnaissance feature in the Road Book and are validated on the passage of the opening car.

4) Between two compulsory passage points, the route described in the Road Book is not obligatory but strongly recommended. Only this route is opened and validated by the opening team.

5) The route will remain secret until the Road Book is distributed to Crews.

6) From 1st May 2012, reconnaissance of the route, logistical reconnaissance (visit of Stage's towns and hotels) and tests giving even a minor advantage are forbidden, in any form, in all the countries to be crossed by the Event, for all the Competitors who have entered or who are intending to enter, and for any person having a link whatsoever with a Competitor entered or intending to enter.

The participation in national races will be the subject of an authorisation made to the Organising Committee of the Organiser, who will decide, according to received information, whether it authorises the participation in these races or not.

Not respecting these rules will lead to the start being refused for the Crews concerned, notwithstanding other possible sanctions imposed by the Competitor's National Sporting Authority.

7) The complete route is checked by the opening team.

8) The distance of the Stages will be given to the Competitors on 21st November 2012 during a press conference organised in Paris.

22P1 ROAD BOOK LEXICON

	ROUTE	ROAD
	ROUTE avec séparateur central	DUAL CARRIAGEWAY
	PISTE TRACEE	TRACK
	HORS PISTE	OFF TRACK
	ATTENTION	1 DANGER
	DANGER	2 DANGER
	GROS DANGER	3 DANGER
	CUVETTE	DIP
	BOSSE	BUMP
	COMPRESSION	COMPRESSION
	SAIGNEE	DITCH
	RADIER	STEP
	MARCHE EN DESCENTE	STEP DOWN
	MARCHE EN MONTEE	STEP UP
	DESCENTE	DOWNHILL
	VERS	TOWARDS
	MONTEE	UPHILL
	TROU EFFONDRE	HOLE COLLAPSE
	ORNIERE	RUTS
	ONDULATION BOSSE	UNDULATION
	SUR PONT SOUS PONT	ABOVE BRIDGE UNDER BRIDGE
	GUE	FORD
	TROU	HOLE
	CAIRN	CAIRN
	CITERNE	WATER TANK
	FIL BARBELE	BARBED WIRE FENCE
	CLOTURE	FENCE
	LIGNE ELECTRIQUE	ELECTRIC LINE
	DUNES	DUNES
	DUNETTE	SMALL DUNE
	PALMIER	PALM TREE
	ANTENNE	ANTENNA / MAST
	PORTAIL BARRIERE	GATE BARRIER

	OUED	OUED / WADI
	LANGUE DE SABLE	SAND SPIT
	BORNE	KILOMETRE MARKER
	CIMETIERE	CEMETARY
	FUT	BARREL
	PANNEAU	SIGNPOST
	MAISON	HOUSE
	FORT	FORT
	POTEAU PIQUET	POST
	PNEU	TYRE
	PUIT	WELL
	RUINES silhouette à dessiner	RUINS individual drawing for each
	MONTAGNE silhouette à dessiner	MOUNTAIN individual drawing for each
	DEBUT de Zone FIN de Zone vitesse limitée	START of Zone END of Zone controlled speed
	Limite de VITESSE	SPEED LIMIT
	STOP	STOP
	DEVERS	CAMBER
	DEPART	START
	ARRIVEE	FINISH
	CHRONO	CLOCK
	ESSENCE	FUEL
	PHOTO	PHOTO
	CP	PC
	DEBUT ZONE D'ASSISTANCE	START OF ASSISTANCE ZONE
	FIN ZONE D'ASSISTANCE	FINISH OF ASSISTANCE ZONE
	VILLAGE	VILLAGE
	WAYPOINT MASQUE	WAYPOINT MASKED
	WAYPOINT ECLIPSE	WAYPOINT ECLIPSE
	A DROITE	ON THE RIGHT
	A GAUCHE	ON THE LEFT
	DROITE	RIGHT
	GAUCHE	LEFT
	SABLE	SAND

	GRAVIER	GRAVEL
	GAUCHE / DROITE	LEFT / RIGHT
	DROITE / GAUCHE	RIGHT / LEFT
	MAUVAIS	BAD
	EMPIERRE	STONY OR ROCKY
	DEFONCE	ROUGH
	ORNIERE	RUT
	SERRE	TIGHT
	HORS PISTE	OFF PISTE / OFF TRACK
	HORS PISTE INTERDIT	OFF TRACK FORBIDDEN
	OUED	OUED / WADI
	ETROIT	NARROW
	DANS	IN
	IMPERATIF	IMPERATIVE
	QUITTER	LEAVE
	GRAND	BIG
	NOMBREUX	MANY
	RALENTIR	SLOW DOWN
	PLUS OU MOINS VISIBLE	MORE or LESS VISIBLE
	SINUEUX	TWISTY
	PISTE PRINCIPALE	MAIN TRACK
	TOUT DROIT	KEEP STRAIGHT
	ROUTE	ROAD
	TOUT DROIT SUR PISTE PRINCIPALE	KEEP STRAIGHT ON MAIN TRACK
	TOUT DROIT SUR ROUTE PRINCIPALE	KEEP STRAIGHT ON MAIN ROAD
	PISTES PARALLELES	PARALLEL TRACKS
	PISTE	TRACK
	CAP	BEARING
	TOUJOURS	ALWAYS
	VEGETATION	VEGETATION
	CAILLOUX	STONE
	EFFONDRE	COLLAPSED
	REPRISE REPRENDRE	TO TAKE

22P2 ROAD BOOK

1) All Crews will receive a Road Book which will indicate the itinerary which has been reconnoitred and which will be 'opened' and 'swept'. It will indicate compulsory points of passage (WPV, WPM, WPE, WPS, DZ, FZ, CP) which must be respected on pain of incurring penalties up to and including Disqualification.

2) A Selective Section or Road Section taking place on an existing track will appear in the Road Book as a continuous line.

A Selective Section or Road Section taking place off-track will appear in the Road Book as a dotted line.

3) It is authorised to carry in the Vehicle, only the route notes for the current Event (official Road Book), the modifications proposed by the opening team, which may be integrated into the Road Book and personal notes resulting from a previous passage (whatever direction) in the current Event.

Maps are allowed inside the Vehicle with the exception of photographic satellite maps.

Crews who do not respect these rules will be refused a start or be disqualified from the race.

4) The Road Book for the first Stage will be given at the Briefing on 4th January 2013 at 09.00pm in Lima.

Those of the following Stages will be given each evening for the next day at the Time Control situated at the finish of the Stage, except during the rest day when the Road Book for the following Stage will be given from 2.00pm.

They will remain the property of the Competitor.

5) Following the passage of the Organisers' opening team, any modifications will be posted each evening before 06.00pm or during the Briefing at the latest, at the Bivouac, under the control and responsibility of the Clerk of the Race (see Article 30P).

6) Competitors must, in all cases, adapt their driving to the condition of the terrain which will change frequently. The greatest attention must be paid at all times, whatever the type of route (Selective Sections, Road Sections, Off-Track, etc.).

23P OFFICIAL ITINERARY

23P1 GENERAL

1) The Official race Itinerary (Selective Sections and Road Sections) is recorded in the GPS(s) supplied to Crews. The chronological validation of each Way Point (WPV, WPM, WPE, WPS, DZ, FZ, CP) localised on the Road Book guarantees the respect of the route by Crews. Each point will be numbered in chronological order in the Road Book and in the GPS.

WPS will be positioned on the route to guarantee the passing of some sensitive points, such as: dangers 3 (!!!), crossings of roads, gaz pipelines, railways, etc.

2) In case of a change of itinerary, the list of compulsory passage points that is downloaded in the GPS may be automatically modified upon arrival at the Bivouac or at the start of the Stage.

It is the Crews' responsibility to ensure that modifications have been carried out.

3) With the exception of the Bivouac's coordinates, no coordinates of Way Points will be indicated in the Road Book.

23P2 RESPECT OF THE ITINERARY FOR T4.3 VEHICLES

1) If a T4.3 Vehicle has carried out assistance on another Vehicle in the race and reaches the Bivouac out of time and by a different itinerary (marked track or tarmac), a penalty of 50 hours will be awarded and will be taken into account for the start order of the following Stage.

The T4.3 Vehicle concerned must imperatively indicate its leaving of the itinerary to Race Control (PCO) via the Iritrack telephone and give the number of the Vehicle on which it is going to carry out assistance, on pain of Disqualification.

2) A T4.3 Vehicle that is too late to take the start, after having carried out assistance and on having informed the PCO by telephone, may be authorised not to do the Stage and to retake the start of the following Stage. This authorisation will be sanctioned by a penalty of 100 hours minimum and will only be given once during the Rally.

3) The Vehicles concerned by this article must respect the rules governing speed limits imposed on assistance category Vehicles. They do not risk being disqualified for missing Way Points.

23P3 PUBLIC ZONES

1) Public Zones will be positioned alongside the route, on one or several points of the Selective Sections, depending on the Stages. They will be intended to regroup the spectators. These zones will feature on the Road Book. Particular vigilance is requested by the Vehicles while crossing these zones.

2) Assistance and any Competitor other than a Crew are not allowed to go there, except if these zones feature on the itinerary of their day's Road Book.

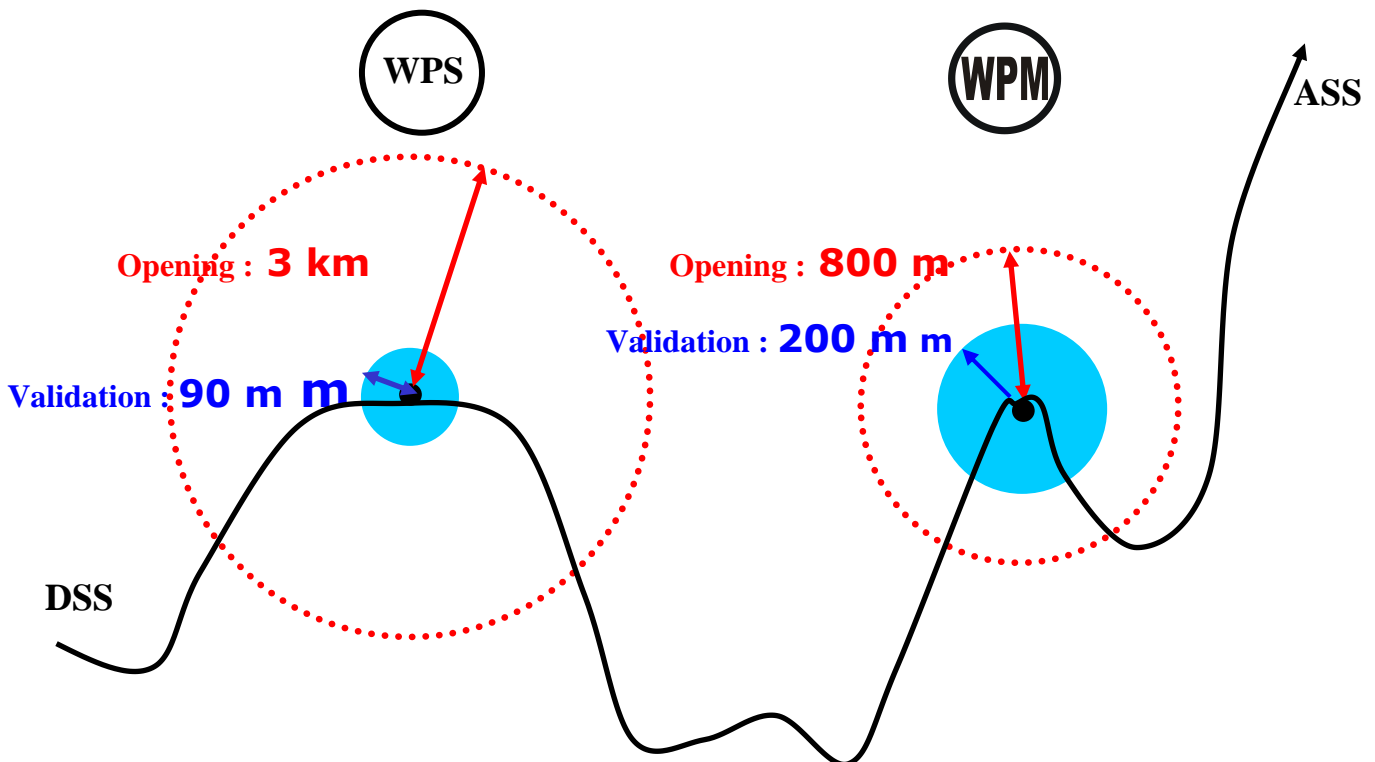
3) Signposting will be forbidden, on pain of penalties that may go as far as Disqualification.

24P FUNCTIONING OF THE GPS

24P1 GENERAL

1) The « GPS » uses the system of Visible (WPV), Hidden (WPM), Safety (WPS) and Eclipse (WPE) Way Points.

To satisfy the navigation regulations, the WPS and WPM work according to the following principle:



2) Between 2 WPM or WPS, the GPS will only show the compass heading and the speed. Once the Vehicle has entered the 800 m radius around a WPM or 3 km radius around a WPS, the GPS will display all the usual functions of a GPS: COG (Cap over Ground), SOG (Speed over Ground), CTW (Cap to Way Point), DTW (Distance to Way Point), etc.

All this information will also be displayed on the screen of the GPS towards a WPE, once the preceding Way Point has been passed. In the case where the Vehicle has not validated the preceding Way Point, the information will be displayed on the screen, once the radius of 3 km around the WPE has been penetrated.

3) Vehicles must respect the chronological order of the Way Points of the Stage concerned. If this is not the case, the GPS will only display the compass heading followed and speed. However, the Vehicle can force the GPS to align itself on another Way Point using the key "W+" or "W-".

24P2 PENALTIES FOR MISSING WAY POINTS

1) The range of penalties for each missing Way Point (WPV, WPM, WPE, WPS, DZ, FZ, CP) will be indicated on the Road Book.

In the case where the Official Itinerary is not respected (succession of missing Way Points representing a short cut), penalties may go up to and including Disqualification, as a function of the profile of the Stage and the number of kilometres missed.

In the case where Way Points are added by the opening team, a new list giving the numbers of Way Points and their penalties will be posted on the official notice boards.

2) In addition to checking the Time Card, an analysis of the GPS and eventually of the Iritrack will be carried out to establish the route actually followed.

24P3 NAVIGATION

1) All navigation equipment of whatever type is controlled, especially GPS. The bringing and possession of any system not explicitly mentioned in this Regulations is forbidden and will result in Disqualification and that irrespective of the mode and technology used to evaluate or estimate ones position.

2) GPS (compulsory on board Vehicles)

The mounting of a GPS of a single model, disqualifying any other type of GPS, supplied by the Organisers' supplier is compulsory.

This equipment must be mounted according to the technical instructions supplied. It is Crews' responsibility to correctly install the necessary mechanical, electrical and electronic elements complying with the safety standards before technical scrutineering and that with the aid of the installation kits purchased from the named supplier. Electrical supply must be permanent, protected by a 3 amp fuse and give a steady continuous current of between 9 and 30 volts. The mechanical mountings must be flexible and use the silent blocks supplied.

This equipment may be fitted twice.

This equipment is personalised and marked with a seal, with a series number attributed to a crew. No changes may be made without authorisation from the GPS supplier. Any exchange of equipment between Vehicles is forbidden on pain of penalties up to and including Disqualification.

3) GPS compass heading, speed and odometer repeater (optional)

A sole model is authorised by the Organisers and must be linked to the fixed GPS. The linking of all other models, or systems (with the exception of Speedox) to the GPS is forbidden, especially to computers or organisers of whatever kind.

This equipment may be fitted twice.

4) Mechanical Tripmeter of the Competitors' choice (compulsory on board Vehicles)

A distance counter linked uniquely to the rotation of the wheels or the transmission shaft. This equipment must not be linked to any other piece of equipment making the use of this information by another piece of equipment possible.

This equipment may be fitted twice.

5) Odometer (function of the GPS Unik)

Counter of total distance, based uniquely on the calculation of the distances covered between each GPS measure (at least one per second). The Odometer is displayed in the ODO and OD+ pages of the GPS Unik.

This equipment can be fitted twice.

6) Magnetic or electronic compass of the Competitors' choice (optional)

Indicator of the compass heading of the Vehicle, based on the earth's magnetic field. This equipment may include an internal electronic compensation system. The readout may be analogical and/or digital. This equipment must not be linked to any other piece of equipment allowing the use of this



information by another piece of equipment. No connection is allowed that might allow the entry or supply of digital data.

This equipment may be fitted twice.

7) General

a) The use of GPS points other than those supplied by the Organiser for Dakar 2013 is forbidden.

Each piece of equipment may perform only one function (compass, odometer, etc.) with the exception of the odometer and compass functions of the race GPS.

Only the linking of the GPS and the GPS compass-heading, speed, odometer repeater (unique models) and Speedox is authorised.

b) All other type of GPS, fixed, portable, integrated, or equipment possessing GPS type capacities, or all other system of navigation by satellite, the recording of the route by any means is forbidden. They can in no case be used or employed during the race.

The carrying or possession of any system not explicitly described in these Regulations is forbidden, and notably all computer systems, electronic navigation aids, computerised map positioning systems, computerised maps scanners or storage devices.

All linking of communication of whatever kind (cable, radio, infra-red, etc.) of different pieces of equipment is forbidden. External communications is also forbidden.

Only the linking of the crew helmets by using a helmet-to-helmet Intercom system is allowed.

c) The presence on board a Vehicle of all non-justifiable wiring is forbidden (electricity supplies, aerials, pick-ups, etc.).

The Organiser reserves the right to carry out physical or electronic test at any moment with the purpose of checking:

- the correct functioning of compulsory equipment;
- the absence or use of forbidden systems;
- the absence of pre-equipment or cabling permitting the later installation of non-homologated equipment.

d) The possession or use of forbidden systems will result in Disqualification.

In the case of doubt concerning the functions of equipment authorised but of the Competitors' choice, their transport may be forbidden.

25P UNBLOCKING THE GPS

For safety reasons, the Crews have the possibility of unblocking the GPS by inputting a specific code:

'WPM' Code:

This code, given by the PCO at the request of the crew via the Irtrack, activates the usual functions of a GPS and makes all the Way Points visible.

All use of this code will result in the following penalties:

- from the 1st to the 3rd utilisation: 6 hours penalties per utilisation for the first 20 in the overall car class, the first 10 overall in the truck class and/or A.S.O. Elite Drivers, 3 hours for other Competitors.
- the 4th utilisation will result in Disqualification for all Competitors.

26P SPEED CONTROL ZONES - DEFINITION

1) Entry of a Control Zone

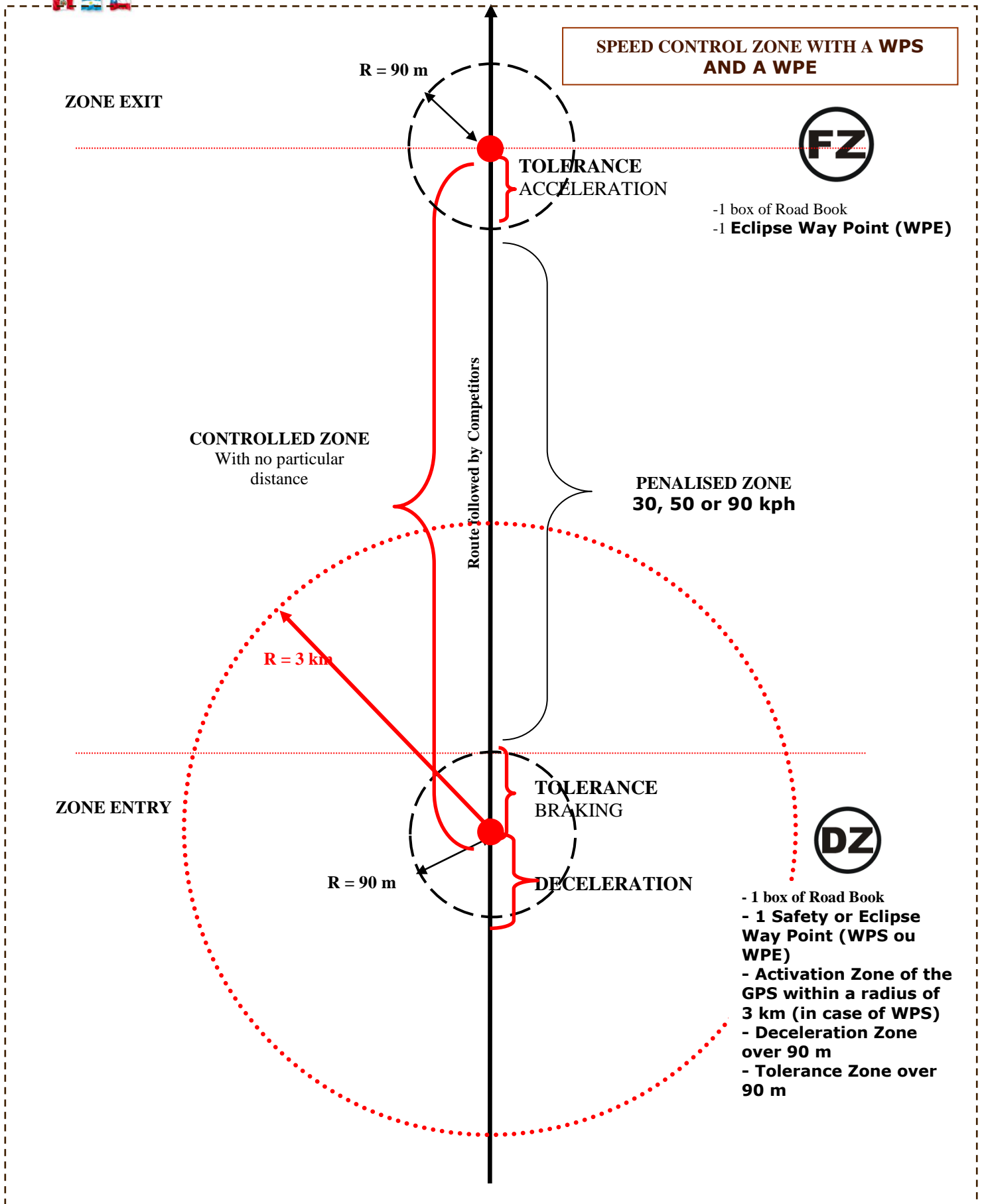
- The start of a Speed Control Zone registered in the GPS will be indicated in the Road Book by a box marked: "DZ" and by a Safety or Eclipse Way Point (WPS or WPE). To validate the entry of Zone "DZ", the Vehicle must pass at less than 90 m (radius around the Way Point "DZ"), on pain of penalties for missing Way Points (Article 24P2);
- In a radius of 3 km of this point (in the case of a WPS) the Vehicle's GPS will become active (DTW, CTW and arrow) so as to guide the Competitor to this point;
- 90 m before this GPS point Competitors will be informed by their GPS that they are approaching a Speed Control Zone (deceleration);
- The 90 m after the GPS point is considered as a deceleration zone (Zone of Tolerance), before entering the Control Zone.

2) The Speed Control Zone

The Control Zone will appear permanently on Vehicle's GPS screens, once the entry Way Point is validated. The Vehicle can in no way claim not to know either the entrance or exit of the zone. The speed of a Vehicle will be limited to 30, 50 or 90 kph between the point of entry and the exit point of the zone, regardless of the route taken between these two points.

3) Exit of the Control Zone

- The end of a GPS Speed Control Zone will be indicated on the Road Book by a box marked "FZ" and by an Eclipse Way Point (WPE);
- Around this point there will be a Zone of Tolerance of 90 m so as to avoid any arguments concerning the measuring of speed;
- Competitors can reaccelerate from this point;
- The exit point of the Speed Control Zone is a compulsory point of passage. To validate the exit of Zone « FZ », the Competitor must pass at less than 90 m (radius around the WPE « FZ ») (penalties: cf. Article 24P2).



27P CONTROL PROCEDURE

1) Throughout the duration of the Rally the Competitor is held responsible for the checking of the working order of the GPS(s) downloaded by the Organisers.

2) The GPS(s) must be working and remain connected permanently with its power supply and aerial plugged in, throughout the entire Leg.

3) Any incident that is the fault of the Competitor (loss, destruction, switching off, etc.) that makes it impossible to read the GPS and/or any attempt at cheating or manipulation, noted by a GPS technician, under the responsibility of the Clerk of the Race, will result in penalties (see Article 29P1.2)

Penalties applied will be identical to those for a Crew who have failed to enter the day's code into their GPS(s).

4) Checks will be carried out at the end of Legs. The Crew must put their GPS on the "CHECK" screen on their arrival at the TC. A page summarizing the infringements appears then on the screen. On stopping to clock in at a Time Control, the information contained within the GPS (missed Way Points, speed, etc.) will be automatically transmitted to the controllers by radio.

In the case of an infringement only, the person carrying out the checks will note any infractions and these will be counter signed by a Crew member or by the Crew. The controller will then hand out a copy to the Competitor and will send a copy to the Race Direction.

If a Crew has two GPS working and only one of the two GPS validates the passage of a Way Point, no penalty will be given.

5) In the case of a protest, accompanied by the appropriate deposit, the Competitor has half an hour after notification to make a written protest to the Clerk of the Race. The GPS(s) will then be dismantled and sealed by a GPS technician in the presence of the Competitor before being examined by a GPS technician who will hand his/her report to the Clerk of the Race and to the Competitor.

6) Control of the speed

In case of speeding, if a Crew possesses two working GPS and the two speeds recorded are different, the Crew is penalised on the lowest speed.

28P TRAFFIC-SPEED

28P1 GENERAL

1) In the event of an infringement of the traffic laws committed by a Crew participating in the Event, the traffic policemen, Judges of Fact or Officials of the Event having noted the infringement must inform the offender thereof as soon as possible. Should they decide against stopping or are unable to stop the Crew in the wrong, they may request the application of the penalties provided for, subject to the following:

- a)** that the notification of the infringement is made through official channels and in writing, before the posting of the classification of the Leg during which the infringement was committed;
- b)** that the statements are sufficiently detailed for the identity of the offending Crew to be established beyond all doubt, as well as the exact place and time of the offence;
- c)** that the facts are not open to various interpretations;
- d)** It is forbidden, under pain of Disqualification:
 - to transport the Vehicles,
 - deliberately to block the passage of the Vehicles, or to prevent them from overtaking.

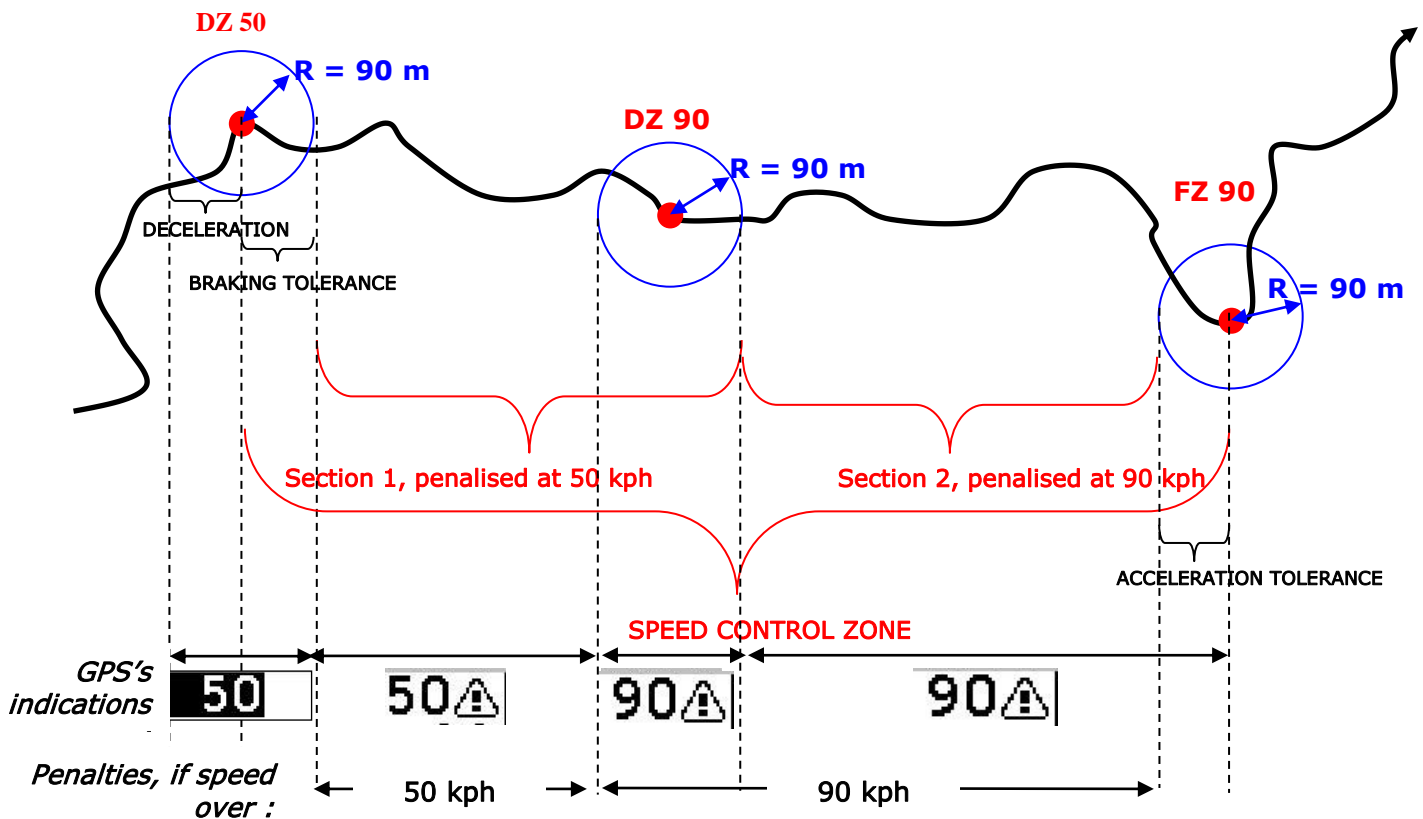
2) In Peru, Argentina and Chile, all Vehicles must have their lights on, when driving on the road. This obligation also applies to the Selective Sections.

28P2 GOING THROUGH SPEED CONTROL ZONES

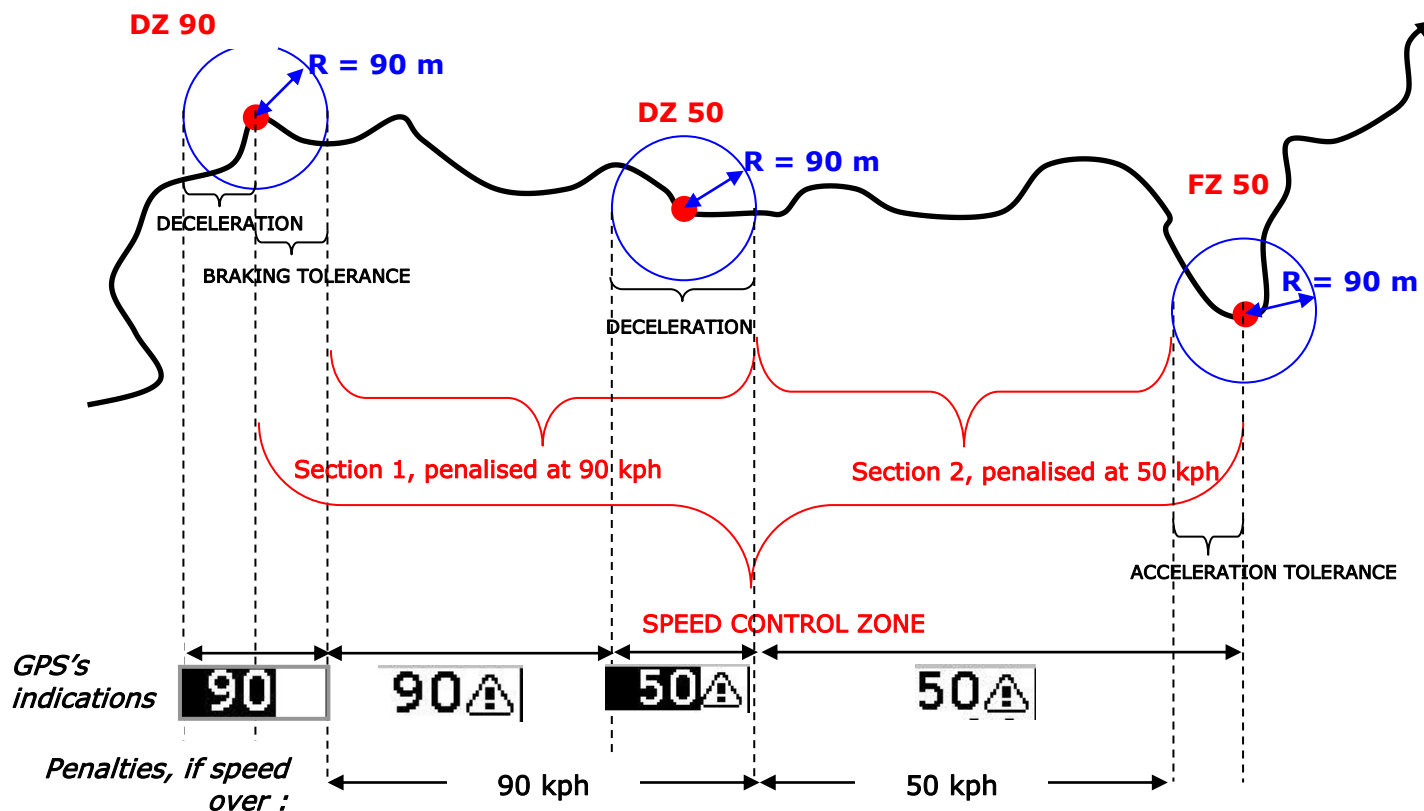
1) In areas defined as Speed Control Zones, the speed of Vehicle both on Selective Sections and Road Sections is limited to 30, 50 or 90 kph.

- 2) If the local speed limit is lower, it applies. Also, it is the Vehicle's responsibility to adapt their speed to local population and traffic conditions.
- 3) Speed Control Zones will be indicated on the Road Book by the initials 'DZ' and 'FZ'.
- 4) The presence or absence of signposts indicating speed limits can on no account serve as an argument in case of dispute. Overtaking is authorised on condition that the maximum speed authorised in the zone is not exceeded.
- 5) In a Speed Control Zone, if the 30, 50 or 90 kph limit is exceeded, a signal will appear on the GPS's screen to indicate excess speed and its recording. The GPS may be checked at the end of the Selective Section and/or upon arriving at the Bivouac, according to exactly the same procedure as described in article 27P.
- 6) When speeding an impulsion is recorded in the GPS at least every 150 m and the speeds shown on the speed page of the GPS "SPD". On arriving at the end of the Selective Section and/or the Bivouac a controller will note all speeding and indicate the offence to the Competitor.
- 7) In the case of a Speed Control Zone composed of two successive sections with two different maximum speeds, the maximum speed authorised in the radius of 90 m around the entry Way Point of the second section will always be the highest speed of the two sections. In the case where the speed decreases, the entry in the radius of 90 m of this same Way Point marks the start of the Deceleration Zone (see diagram below).

2 SUCCESSIVE SECTIONS, INCREASING SPEEDS: 50 to 90 kph



2 SUCCESSIVE SECTIONS, DECREASING SPEEDS: 90 to 50 kph



28P3 PULSE SIGNAL (IMPULSION)

1) If the Competitor disagrees with the infringements noted, they must make a written protest, accompanied by a deposit, which they must then hand to the Race Direction within a half hour of notification, so that the GPS can be further examined.

2) Any speeding recorded by the GPS will be penalised by the Clerk of the Race, as follows:

- between 1 and 15 kph :
1 minute x the number of impulsions + a fixed amount of €100;
- between 16 and 40 kph :
2 minutes x the number of impulsions + a fixed amount of €200;
- more than 41 kph :
 1st impulsion: 5 minutes + a fixed amount of €300;
 2nd successive impulsion: 10 minutes + a fixed amount of €500;
 3rd successive impulsion: 15 minutes + a fixed amount of €700.

3) All repeated offences will result in penalties from €1,000 to Disqualification.

4) NB: Fines must be paid to the Competitors' Relations Service or the Clerk of the Race, within 48 hours after notification, on pain of being refused a start.

28P4 SPEED LIMITS – ROAD SECTIONS

1) Independent of Speed Control Zones (Article 28P2), the maximum speed will be limited and checked on all Road Sections, except in special cases specified in a supplement and/or in the Road Book.

The maximum speed allowed will be that of the country crossed as a function of the type of Vehicle:

- Peru **100 kph** for cars and **80 kph** for trucks,
- Argentina: **110 kph** for cars and **90 kph** for trucks,
- Chile: **110 kph** for cars and **90 kph** for trucks.

2) On Road Sections an impulse will be recorded in the GPS every **500 m**. A sign will be displayed on the GPS screen to indicate the recording of a speeding offence. The GPS will be checked on arriving at the Bivouac according to the procedures detailed in article 27P.

3) All speeding recorded in the GPS will be sanctioned by the Clerk of the Race by the following penalties:

- **From 1 to 20 kph:**
30 seconds per impulse + a fine of €100;
- **From 21 to 40 kph:**
1 minute per impulse + a fine of €200;
- **more than 41 kph:**
5 minutes per impulse + a fine of €300.

Over 3 successive impulses, penalties to be decided by the College of Sporting Stewards, up to Disqualification.

28P5 TRUCK SPEED LIMIT

1) On Selective Sections, the speed of racing trucks is limited to 140 kph.

2) All speeding recorded by the GPS will result in the following penalties:

- Between 141 and 145 kph:
1 minute x number of impulsions + a fixed fine of €100;
- Between 146 and 160 kph:
5 minutes x number of impulsions + a fixed fine of €200;
- Over 161 kph:
1st impulsion: 20 minutes + €300,
2nd impulsion: 1 hour + €1,000,
3rd impulsion: Disqualification.

29P DRIVER'S CODE

29P1 SENTINEL FUNCTION OF GPS

1) With a view of making overtaking safer, the Sentinel function of the GPS (a device that signals to a Vehicle that he can be overtaken) is compulsory for all categories.

2) The GPS - Sentinel must be in operation throughout the running of each Leg and must be connected directly to the battery of the Vehicle. The operation of the GPS - Sentinel is the responsibility of the Competitor.

If it is noted that the GPS - Sentinel is not in operation, through the fault of the Crew, the following penalties will apply:

- 1 hour for Vehicles classified among the top 20 in the general car classification, the top 10 in the general truck classification, and/or A.S.O. Elite Drivers;
- €300 for the other Vehicles.

3) All Vehicles caught by another must do whatever is necessary to pull over and allow themselves to be overtaken.

In a Selective Section, any Vehicle running which has received several audible warnings within a given time and which has not pulled over to allow the other Vehicle to overtake may be sanctioned at the Sporting Stewards' discretion, following examination of the downloaded data:

- 15 minutes for Vehicles classified among the first 20 overall in the car class and for the first 10 overall in the truck class and/or A.S.O. Elite Drivers;
- €300 for other Vehicles.

4) All contested cases will be treated by the College of Sporting Stewards, following an audition with the two parties concerned. Depending on the circumstances they may apply other penalties (time or financial), notably to the best placed Crew of the Team of the Crew at fault.

5) In case of dispute, the data from the Sentinel is downloaded, after a written request to the Clerk of the Race, 30 minutes at the latest after the infraction has been transmitted.

29P2 ACCIDENT

1) In the case of an accident involving injuries, it is imperative that Competitors inform the Race Control (PCO) by all possible means as quickly as possible so that appropriate means can be sent as quickly as possible.

2) As Crew, if at least one of its members is conscious and capable of moving:

- make the area safe by simultaneously pushing the two blue buttons on the GPS, so as to activate the Sentinel alarm function and inform other Competitors arriving in the area;
- push the red button on the Iritrack to inform the PCO of the accident;
- push the blue button on the Iritrack to be able to communicate with the PCO and inform them of the situation.

In the case of the Iritrack malfunctioning, if the Competitor cannot communicate with the PCO, they must let off their distress beacon.

3) If the Vehicle represents a danger to other Vehicles or, to make safe the area of the accident, a Crew member must, after having pressed the 2 blue alarm buttons on the GPS, place a red reflective triangle in an appropriate position, at least 50 metres before the Vehicle, so as to warn other Crews. All Crews failing to respect this rule are liable to penalties at the discretion of the Sporting Stewards.

4) Any Crew involved in an accident which results in physical harm or not, can be subject to an investigation by the College of Sporting Stewards. Depending on the circumstances, penalties may be applied, including Disqualification.

5) Any Crew which fails to comply with the prescriptions of the present Article 29P2 will be reported to the Sporting Stewards who may impose penalties as provided for in the Code.

29P3 ASSISTANCE IN THE EVENT OF AN ACCIDENT OF AN OTHER VEHICLE

1) It must be remembered that ethics demand that a Crew which sees that an accident has occurred must stop in order to provide assistance in the most appropriate manner until the rescue service arrives.

Also, it must be remembered that major means are implemented to shorten the intervention times.

2) Any Crew that witnesses an accident placing another Crew in physical danger must in the following order:

- stop;
- make the area safe by pushing simultaneously the two blue buttons on the GPS so as to activate the Sentinel's alarm function, to inform the Crews who arrive on the spot;



- press the red button on their Iritrack;
- give first aid to the Crew members and get information on their condition;
- call the Race Control (PCO) via their Iritrack (blue button) to report the situation;
- wait for the rescue service or another Vehicle to arrive;
- press the green button on their Iritrack, to signal that they are leaving the scene.

If it is impossible to communicate with the PCO via the Iritrack, the Crew arriving at the scene of the accident must let off the Sarsat distress beacon of the crashed Vehicle, or if this cannot be found, their own distress beacon.

The Crew who witnesses the accident, do not risk being disqualified if they let off their own Sarsat distress beacon for the injured Crew.

3) The total stopping time (if more than 3 minutes) between the two Iritrack alerts (red button on arriving and green on restarting) will be subtracted from the time taken to cover the Selective Section on the same day, but only for the first two Crews to stop at the scene of the accident, upon the request of the Competitor to the Clerk of the Race, made within a maximum of 30 minutes after the Crew finishes the day's Stage.

The total stopping time can be checked and validated by the Iritrack and/or by the GPS Unik.

4) Any Crew which fails to comply with the prescriptions of the present Article 29P3 will be reported to the Sporting Stewards who may impose penalties as provided for in the Code.

29P4 DRIVERS' CODE

1) General

a) Competitors and Crews must behave with respect both on the route and with regard to:

- citizens in the countries that are travelled through;
- other Competitors;
- the Organisers.

Any impoliteness which is proven will be subject to a penalty of €500.

b) Any incorrect, fraudulent or unsporting action carried out before or during the Event by the Competitor or members of the Crew will be judged by the Sporting Stewards, who may impose a penalty which can go as far as Disqualification or allow an organiser to refuse the entry of the incriminated Competitor(s) or to refuse them the start.

c) It is forbidden to leave wheels and/or punctured or damaged tyres on the itinerary of the route. Any Competitor caught disobeying this rule will be penalised €1,000 per tyre or wheel. Repeat offending will result in penalties up to and including Disqualification.

2) Sale/transfer of Vehicle

If a Crew withdraws or at the end of the Rally, it is strictly forbidden transferring or selling its Vehicle in any of the countries crossed.

In addition to the risks incurred locally by failure to comply with the rule in force (fines, impoundment of Vehicle/passengers, etc.), any Crew found to have breached this rule will not be permitted to enter the Dakar Rally for 5 years.

3) Crossing the Peru/Chile, Chile/Argentina and Argentina/Chile borders

For health reasons, it is strictly forbidden to take the following items from Peru to Chile, from Chile to Argentina and from Argentina to Chile (SAG, act n°18755 1989, modified in 1994, act n°19283 – SENASA resolution n°295 in 1999 and resolution n°816 in 2002). Vehicles will be searched prior to crossing the border.

Prohibited products:

- Fruit, vegetables, herbs, plants, tubers, bulbs, grains, hay, twigs, soil, cut flowers, handicrafts, wood and other vegetable products, pesticides and fertilisers;
- Animals, birds, bees, honey, cheese, milk, meat and dairy or meat products, embryos, seeds, veterinary medicines (vaccines, diagnostic kits, antibiotics, medicated shampoo, etc.) or any other animal products or any animal food;

- Protected flora and fauna and/or by-products derived from species under threat of extinction, including threatened species of flora and fauna under the Convention on International Trade in Endangered Species (CITES).

In addition to the risks incurred locally by failure to comply with the rules in force (fines, impoundment of Vehicle/passengers, etc.), any Crew found to have breached this rule will incur penalties going from a €500 fine up to Disqualification.

4) Mandatory steps with regard to customs formalities

a) Packing List

- The Packing List is defined as the mandatory and official administrative document of the temporary importation of each Vehicle, whatever category it is registered in, race and assistance;
- The Competitor must permanently keep this document with him; the Competitor must give a copy to the Rally customs declarant as well as to the Organiser during administrative checks, and/or at the embarkation in Le Havre;
- The Packing List is a mandatory document to be able to cross borders. It gives a detailed list of all the goods and material carried on board of each Vehicle including information on the Vehicle itself. This list must be scrupulously identical from the moment it leaves Europe and/or when it arrives in Peru to the passage of the last border AFTER the Event marked by the return to the country of origin.

b) Transportation of spare parts

- In order to comply with customs requirements, and to ensure there are no problems in crossing borders, or when embarking: all borders must be crossed with the same spare parts in the Vehicle as declared at the outset, and detailed in the mandatory Packing List of each Vehicle. It is therefore strictly forbidden to sell or transfer any items being transported;
- In addition to the risks incurred locally by failure to comply with the rules in force (fines, impoundment of Vehicle/passengers, etc.), any Crew found to have breached this rule will be fined €500 per item (e.g.: one tyre = €500). Any subsequent offence may incur penalties, including Disqualification.

c) Steps to follow when withdrawing

In case of an assistance Vehicle's withdrawal, its Crew will ABSOLUTELY have to, on top of complying with the Article 15.2 requirements of the Assistance Regulations, tell as soon as possible the customs declarant as well as the Competitors Department about the Vehicle state, its position and the steps taken to transport the Vehicle back to: either San Antonio port (in Santiago, Chile) to embark in direction of Le Havre, either to the Vehicle's origin country if it has not embarked in Le Havre on the Organiser's ferry.

5) Loss of Vehicles (complete or partial destruction)

If a Vehicle is lost (accident, fire, destruction, etc.), the Competitor must make a statement to the police or customs. This statement must contain the facts of what happened, the registration number, the race number, the contact details of the people involved and of the witnesses, etc. A copy of this statement must be provided to the Organisers.

Then, the Competitors must approach the Organisers to find out what to do.

The Vehicle must be returned immediately to the port of San Antonio in Santiago (Chile) where the local authorities will decide, depending on the state of the Vehicle, whether it should be re-exported, destroyed, or whether cancellation of its temporary import is justified. Any additional repatriation costs must be paid by the Competitor.

In addition to the risks incurred locally by failure to comply with the rules in force (fines, impoundment of Vehicle/passengers, etc.), all infractions to this rule will lead to a request for sanctions to be imposed by the Sporting National Authority of the Competitor concerned.

In the case of damaged Vehicles that came on the road, the Competitors must approach the Organisers to find out what to do.

6) Respect sensitive areas

a) In agricultural or forest areas, populated zones or zones that are sensitive from an environmental or safety stand point, the itinerary and all the boxes of the Road Book must be scrupulously followed. In particular it is forbidden to 'cut' corners by crossing fields, forests, orchards or marshes.

- 1st infringement : €1,000 + 15 minutes,
- repeated offence: Disqualification.

b) So as to respect the crossed areas, it is forbidden to destroy the fences' gates located along the route. Judges of Fact will be on spot.

All broken fence gates will result in a penalty of €1,000 + 15 minutes.

All repeated offence will result in Disqualification.

c) Certain areas of the itinerary are declared SENSITIVE ZONES. They will be mentioned on the eve of the Stage concerned, at the Briefing and on the Briefing notes. They will be indicated on the ground by posts and 'plastic tape'.

A specific logo representing these sensitive areas will feature on the third column of the Road Book (Articles 29P4.6a and c).

All Crews failing to respect these sensitive zones will be penalised in the following manner:

- 1st infraction : €1,000 + 15 minutes,
- repeated offence : Disqualification.

29P5 SATELLITE TELEPHONE / GSM

1) For safety reasons, the presence of an Iridium satellite telephone and/or a GSM phone (except PDAs or GSM phones featuring a GPS function or maps) on board Vehicles is authorised.

Attention, network coverage may be very weak in places. It is preferable to be equipped with an Iridium satellite phone.

2) The number(s) of all SIM cards inside the car, for whatever type of communication system must be given to the Organisers during administrative checks. Except in cases detailed in 3) below, in no case may telephones be in 'on' mode during Selective Sections. Spot checks will be made.

3) On Selective Sections, only in case of problems may telephones be used, only outside of the Vehicle, with the Vehicle stopped, to signal a withdrawal, an accident or a breakdown. Beforehand, the crew will have to inform the Race Control (PCO) of their situation via their Iritrack.

4) These telephones may be used from inside the car, only on Road Sections and only by the co-driver.

5) No permanent aerial, 'hands-free' kit, fixed installation or pre-wiring is authorised in the Vehicle, except for the GPS(s) and tracking systems supplied by the Organiser.

6) During Selective Sections, no transmissions (to or from the Vehicle) of the type SMS, MMS, or of data is authorised. All equipment (data cables, infra red, Blue Tooth, Wi-Fi or others) is forbidden.

7) Only the mode telephone is authorised. Spot checks will be made.

8) All infractions will result in penalties up to and including Disqualification.

29P6 RADIO AND TRANSMISSIONS

1) Only the following aerials will be authorised:

- for the race GPS, supplied by the Organiser's supplier;
- Iridium linked to the Iritrack and supplied by the Organiser's supplier;
- rdio aerials designed only to receive AM or FM public radio broadcast on authorised wavebands.

To the Disqualification of all other type of aerial, linked or not, of type Standard C, D+, M, mini M, Argos, radios, telephones, etc. All infractions may result in Disqualification from the race.

2) Radios

All HF-VHF-UHF-CB transmitters and/or receivers or all other means of communication are forbidden throughout the entire Rally itinerary aboard Vehicles in the race. AM/FM radio receivers must be commercially available models and not modified. The wave length of FM receivers is limited to 88 – 108 Mhz. Spot checks will be made. All infractions will result in penalties up to and including Disqualification.

Only Walkie Talkies operating on one single frequency, used within the confines of the Bivouac are admissible. If they are not supplied by the Organiser's radio supplier (TDCOM), an authorisation must be demanded from the authorities of the countries crossed and the frequencies used must be given to the Organisers.

3) Satellite links

All satellite links or other links between a race Vehicle on a Selective Section and an exterior base or another Vehicle, other than the Iritrack and the Sarsat distress beacon, are forbidden.

4) Data

All data transmission systems, that allow the tracking of Vehicles and management of Vehicle fleets is forbidden.

And that whatever the means or technical system used, on the pain of Disqualification, with the exception of Iritrack and Sentinel.

29P7 IRITRACK

1) The Iritrack is a system that allows Vehicles to be followed by satellite, provided by the Organisers and compulsory for all Competitors.

The alarms and alerts may be let off either automatically or manually.

a) automatic mode:

- alarm following a violent shock (deceleration meter), followed by a stop of 3 minutes;
- abnormal inclination (inclination meter) followed by a stop of 3 minutes.

b) manual mode:

- blue button: phone call to the Race Control (PCO);
- red button: accident with injuries;
- green button: accident without injuries or breakdown.

c) Alternatively, at any moment, when in doubt, the Race Control (PCO) can contact a Competitor by telephone.

2) Throughout the Rally, Competitors are responsible for the correct functioning of their Iritrack. It must be functioning and stay permanently connected, with power cables and aerial connected throughout the length of each Stage. Any incident caused by the Competitor (loss, destruction, being switched off, etc.) and/or all attempts of a fraudulent nature or manipulation will result in penalties, up to Disqualification, to be decided upon by the College of Sporting Stewards.

29P8 ON BOARD CAMERAS

1) Competitors are obliged to accept the fitting of a kit (power cables and mounting bracket) and the fitting of on board camera (+ sound) during the Rally. These systems will be installed temporarily in Vehicles as required by the Organisers. All refusal will result in the start being refused.

2) The camera will switch on automatically during Selective Sections and/or Road Sections. Competitors will be informed that cameras are functioning via a light. The camera must function and remain permanently connected, with power cables and aerials connected, throughout the Stage.

3) All incidents caused by Competitors (loss, destruction, switching off, etc.) and/or all attempts at fraud or manipulation will result in penalties to be decided upon by the College of Sporting Stewards.

4) No system for on board images will be authorised on board the Vehicles, except the on board camera fixed by the Organisers' supplier.

30P RECONNAISSANCE AND OPENING OF THE ROUTE

30P1 RECONNAISSANCE

- 1) The Organiser's reconnaissance crew will verify that the route is accessible to all race Vehicles.
- 2) The reconnaissance crew shall consist of persons showing significant experience of Cross-Country Rallies.

30P2 OPENING OF THE ROUTE

- 1) The Organisers undertake to put in place a team, whose purpose is to check the entire route, a few days ahead of the race, under the responsibility of the Assistant Clerk of the Race.
- 2) The opening of the route will be done under the responsibility of an Assistant Clerk of the Race managing the group and with the help of the designer of the official route. The driver of the opening car must have a significant background as an experienced driver of competition T1 cars on Cross-Country Rallies.
- 3) The opening vehicles are equipped according to the "Reconnaissance and Route Opening Guidelines" of the FIA.

31P ASSISTANCE AND REFUELLING

31P1 GENERAL

- 1) During Selective Sections, any assistance except that expressly allowed in the current Regulations is forbidden (except between Crews still in the race).
- 2) **During the entire duration of the Rally, assistance carried out by assistance personnel is allowed only on the sections common to the competition and to the service Vehicles. The assistance carried out in the Selective Sections is strictly prohibited.**
- 3) **Assistance crews must keep exactly to the itinerary of Road Sections and service areas such as detailed in the Road Books which are available from the Organiser.**
- 4) **All infractions of the assistance Regulations will incur penalties up to and including Disqualification. All assisted Crews are responsible for their assistance.**
- 5) **A Crew may be temporarily or permanently deprived of their assistance Vehicle and the material it transports, depending on the gravity of the infraction committed.**

31P2 AUTHORISED ASSISTANCE

- 1) Only assistance Vehicles and people officially entered in the race or as assistance are authorised to transport assistance materials that must weight no more than the weight limit imposed by the International Highway Code.

2) Assistance is authorised

a) On the route of a Selective Section

By the Crew of a car, a bike or a truck officially entered in and still in the race.

By an assistance category vehicle, after the closure of the control at the end of the Selective Section and after having informed the Organiser.

b) On the route of a Road Section

By the Crew of a car, a bike or a truck officially entered in and still in the race. By vehicles in the assistance category, when the assistance vehicles have the same itinerary to that of the race. Refuelling with petrol or diesel out of an assistance vehicle is prohibited.

c) Between the end of a Stage and the start of the following Stage (at the Bivouac)

By the Crew of a car, a bike or a truck officially entered in and still in the race and by vehicles in the assistance category.

It is authorised for persons entered as assistance to freely take race Vehicles out of the Bivouac for the purposes of refuelling, washing or for testing within a 30 km radius of the Bivouac. (the Iritrack must be switched on).

d) At the Bivouac, after the Start of the Selective Section

Once the Vehicle has taken the Start of the Selective Section, assistance at the Bivouac (under conditions described in point c) is authorised providing the Vehicle does not return to the Bivouac in the opposite direction to the Rally itinerary.

Where there is only one track, returning to the Bivouac is forbidden, on pain of Disqualification.

In the case of off-track, for safety reasons and so as not to meet Vehicles coming in the other direction, Competitors must move away from the tracks to the Bivouac, without incurring penalties.

e) In authorised zones

Assistance zones may be allowed and will be shown in the race and assistance Road Books.

f) 100 % electric Vehicles will be allowed to change their battery at the bike refuelling, and at the Start and Finish of Selective Sections. This change will be made by an accredited Vehicle which must not transport any other parts than those necessary to change batteries. The Organisers must be informed of all points corresponding to a change of battery 48 hours in advance.

31P3 PENALTIES FOR FORBIDDEN ASSISTANCE

1) A Team put in place by the Organisers on the itinerary and at the Bivouac will be dedicated to checking all forms of forbidden assistance.

2) Assistance carried out by a non-accredited Vehicle or person will lead to the following penalties :

- **1st infringement : obligation for Vehicles and/or the persons concerned to be accredited with the Organisation or to leave the itinerary of the Rally + a 3-hour penalty for the Competitor concerned, 6 hours on a Selective Section;**
- **2nd infringement: Disqualification of the concerned Crew.**

3) Carrying out assistance on a Selective Section (Vehicle and/or person registered in the assistance category) will lead to penalties from 6 hours to Disqualification. All repeated offences will lead to Disqualification.

4) Any presence of an assistance on a Road Section, a Start or a Finish of a Selective Section when they do not feature on the assistance Road Book, will lead to penalties from 3 hours to Disqualification. All repeated offences will lead to Disqualification.

5) Any transport of parts or assistance by a Vehicle not accredited, the dropping or parachuting of spare parts will lead to penalties from 6 hours to Disqualification. All repeated offences will lead to Disqualification.

6) Carrying out assistance outside the Bivouac or in an enclosed place (inside or outside the Bivouac), will lead to penalties from 6 hours to Disqualification. All repeated offences will lead to Disqualification. A tent is not considered as an enclosed place.

7) Signposting or the transmission of information by any means will be sanctioned by a 3 hours + €500 penalty per infringement, for the concerned Vehicle.

8) The presence of any means of transport (car, bike, truck, aircraft, helicopter, etc.) following the Rally route the same day or a number of days prior, as well as the prolonged flying over of any means of transport, with official means or not, carrying aboard a person having any link whatsoever with Competitors still in the race will lead to the immediate Disqualification of all the participants having a link with one of these means.

9) All airborne assistance not controlled by the Organisers will lead to penalties from 6 hours to Disqualification. All repeated offences will lead to Disqualification.

Airborne assistance is considered as any presence on a Leg of an aircraft having aboard any person with any link whatsoever with a race Vehicle and any transport of assistance materials (tools, generators, lights, compressors, etc.) in private planes or planes chartered by the Organisers.

31P4 TEAM MANAGERS

1) Teams Managers are allowed to go to the Finish of Selective Sections, when these feature on the assistance Road Book, or when they are located at less than 50 km of the Bivouac. To do so they must ask for authorisation from the Organisers giving the name of the Team Manager and the type of Vehicle to be used. At no moment can they be on the itinerary of the Selective Section, except if specifically authorised by the Clerk of the Race. Team Managers are not authorised to transport tools and spare parts in the Team Manager's plane. In the case of infraction, the assisted Competitor will receive penalties up to and including Disqualification.

2) Within the same Team, it will be possible to enter one to several individuals as Team Manager and they may alternate between the Team Manager aircraft and the assistance Vehicles. No change of name may be made during the race, except in the case of great circumstance, that has been judged and accepted as such by the College of Sporting Stewards.

3) The Team Manager must, during scrutineering, approve and sign the list of Vehicles in the assistance category which have been entered for them and for which they are sportingly responsible.

4) Team Managers, corresponding to the criteria of Article 3P33, may benefit, where possible, from the Organisers aircraft transport. A.S.O. reserves the right to accept or not any person as Team Manager.

5) Entry is by invitation only, upon the reception of requests, which must be received by the Competitors Department before 1st October 2012. They must be sent to:

Dakar Competitors' Department
Tel: +33 (0)1.41.33.14.60 Fax: +33 (0)1.41.33.14.69
e-mail: concurrents@dakar.com

31P5 FUEL/AUTONOMY

1) The allowed fuels will have to match the following norms:

- **FIA norms, Appendix J;**
- **official norms of fuels distributed in the petrol stations in Peru, Argentina and Chile (norms available from the Organisers).**

The maximum limit of CETAN for Diesel Vehicles is 55.

The use of special fuels, other than those described above is strictly forbidden on the pain of Disqualification, with the exception of Vehicles racing for the Alternative Energies Challenge.

2) The use of aviation fuel (AVGAS) is authorised for petrol-engine Vehicles.

The use of bio fuels will be accepted upon declaration, after acceptance of the file by the Organising Committee. The requests have to be carried out before 15th October 2012.

3) All Vehicles must have a minimum range of 800 km, with the exception of 100% electric Vehicles. Each Competitor is responsible for calculating their fuel range. In no case may Competitors make any claims against the Organisers if their Vehicle fails to cover the minimum distance of 800 km, regardless of the nature of the terrain.

For safety reasons, a 10% margin is indispensable, i.e. a range of 880 km.

4) The obtaining of fuel is authorised uniquely, on pain of Disqualification:

- **at commercial service stations in the Stage towns or on the Road Sections;**

- via distributors authorised by the Organisers (list available upon request) at the Bivouac on the designed area;
- at the Organiser's distribution points (bike refuelling for NRJ Challenge Vehicles).

5) Refuelling out of barrels is forbidden on Road Sections. Refuelling is authorised on Road Sections only at the petrol stations, only with local fuel, available from the pump of the petrol station. Fuel delivery is allowed only at the Bivouacs or at the petrol stations of the towns of the Bivouacs (Leg town).

6) When there is no petrol station close to the Bivouac, the Organiser will make fuel available at the Bivouac, which will be delivered in barrels (Diesel and 95 octane lead-free). Order forms will be sent out by the Organiser in September and must be returned by mid-October 2012. Tickets corresponding to orders will be given to Competitors at administrative checks so that Competitors may obtain fuel at the Bivouacs concerned.

7) For safety reasons, refuelling at the Bivouac can only be done in areas designed and signposted by the Organiser. The Competitor must have a fire extinguisher within arm's reach and be at a respectable distance from other Vehicles.

The responsibility during refuelling is the Competitor's alone. Engines must be stopped during all refuelling.

The Competitor must remain outside the Vehicle during refuelling.

8) Refuelling between racing Vehicles is authorised.

9) The filling of fuel tanks of T1 and T2 diesel cars can only be made from T4 or T5 trucks, if the fuel tank of the truck is located outside the bodywork of the truck, under the Vehicle. T5 trucks can only refuel at the Bivouac.

10) The failure to respect the clauses listed above will result in penalties up to and including Disqualification. Checks will be made.

31P6 RACE VEHICLE BECOMING AN ASSISTANCE VEHICLE

1) Crews disqualified or having withdrawn must leave the Rally and remove their race numbers and "Rally" plates.

They may continue on to Santiago, through the daily Bivouacs, as an assistance Vehicle after having:

- informed the Race Direction, via the Crews' Relations Officers, or via Iritrack phone;
- contacted the person in charge of the assistance category;
- modified their door and roof race plates;
- obtained the assistance Road Book;
- exchanged their crew's bracelets for assistance bracelets,;
- returned their Sarsat distress beacon and their Iritrack to the suppliers;
- returned their race GPS (ERTF). Obligation to rent a GPS from the supplier Tripy.

2) They must neither drive nor stop on the race itinerary on the same day as its passing, nor on days preceding its passage. They must respect the instructions stated in this document and conform to the assistance vehicle Regulations.

3) Failure to respect these clauses will result in the request for sanctions from the entrants' National Sporting Authority.

32P ENGINE CHANGE

In Group T1, only one engine block is authorised per Vehicle.
Any change of the engine block will lead to Disqualification.

33P1 ASSISTANCE/REPATRIATION

The Organiser has passed assistance/repatriation services to MUTUAIDE, a subsidiary of Groupama, in the case of:

- physical attack which makes it impossible, in the opinion of a member of the Rally medical team, to continue with the competition;
- death;
- hospitalisation or death of a family member;
- legal proceedings;
- psychological trauma.

1) DEFINITIONS**a) Insurer**

MUTUAIDE ASSISTANCE, hereinafter referred to as MUTUAIDE, a limited liability company with subscribed capital of €9,590,040, headquartered at 8-14 avenue des Frères Lumière, 94368 Bry sur Marne Cedex, regulated by the Code of Insurance –under the control of the “Autorité de Contrôle Prudentiel” located on 61, rue Taitbout – 75009 Paris – under 383 974 086 R.C.S. Créteil.

b) Beneficiaries

- Competitors and their assistance;
- members of the Organisation, their suppliers and their employees, their partners and their employees;
- Event Officials;
- journalists and members of the media;
- persons invited by the Organisers, their partners and anyone participating in a trip organised by V.S.O.

c) Area of applications

Cover is provided for the DAKAR 2013 Rally course from the first day of administrative checks and scrutineering in Lima (02/01/2013) through to 22nd January 2013 at midday in Santiago and covers Competitors and their assistance.

During this period, Competitors who leave or abandon the competition will continue to be covered in Peru, Argentina and Chile provided they continue to follow the Rally, or rejoin Santiago or the port of San Antonio Valparaiso taking the most direct route from the point at which they abandoned the Rally.

d) Domicile

The Beneficiary's usual place of residence. In the case of legal proceedings, the address for tax will be deemed to be the place of domicile.

e) Europe

Countries of the European Union

(Germany, Austria, Belgium, Bulgaria, Cyprus, Denmark, Spain, Estonia, Finland, France, Greece, Hungary, Ireland, Italy, Latvia, Lithuania, Luxemburg, Malta, the Netherlands, Poland, Portugal, The Czech Republic, Romania, the United Kingdom of Great Britain and Ireland, Slovakia, Slovenia, Sweden.)

f) Family

Spouse not separated or divorced, legal cohabitant or civil partner, children, direct ascendants.

g) Physical attack

Any deterioration in health following an accident or sudden illness and/or unforeseeable.

2) COVER

REMEMBER

In the case of physical attack, the Rally medical team will arrange and organise transport of the Beneficiary from the place of the accident to the Rally Bivouac or to the nearest appropriate medical institution using the Rally's ground or air transport.

HOW THE COVER IS PROVIDED

From the Bivouac or medical institution to which the Beneficiary has been transported by the Organiser, the Medical Director of MUTUAIDE, in liaison with the Rally medical teams, **will decide, purely on the basis of medical interests and in compliance with the health regulations in force:**

In the case of a Beneficiary resident or domiciled in Europe (see countries designated above):

- either to hospitalise the Beneficiary in a nearby care centre, if their medical condition requires it, before considering return to a European institution close to, or in, their place of domicile;
- or to arrange and organise transportation of the Beneficiary to their place of domicile or to appropriate hospital facilities close their place of domicile.

In the case of a Peruvian, Argentinean or Chilean Beneficiary:

- either to hospitalise the Beneficiary in a nearby care centre, if their medical condition requires it, before considering return to an institution close to their place of domicile, or in their place of domicile;
- or if the Beneficiary is physically incapable of travelling by their own means, to organise the transport of the beneficiary to their place of domicile or to an appropriate medical structure close to their place of domicile.

In the case of a Beneficiary domiciled outside Europe and outside the three host countries of the Rally:

- either to hospitalise the Beneficiary in a nearby care centre before, if necessary, if their medical condition requires it, considering return to a European institution;
- or to arrange and organise transportation of the Beneficiary to a European city or to appropriate European hospital facilities.

Any request for immediate repatriation to a country outside Europe will be examined on a case by case basis with the Medical Director or MUTUAIDE.

If necessary, MUTUAIDE will carry out local research to find a suitable medical facility.

Information from the regular GP, which is often important, can help the Rally medical team to reach the most appropriate decision.

It is, in this respect, expressly agreed that the final decision, taken purely on the basis of medical interest for the Beneficiary, rests ultimately with the Medical Director of MUTUAIDE.

The final choice regarding hospital location, date, the need for the Beneficiary to be accompanied and the methods used will be taken exclusively on the basis of medical considerations.

If the Beneficiary refuses to abide by the decision which the Medical Director deems most appropriate, he expressly frees MUTUAIDE from all responsibility in particular if he returns by his own means or if his state of health deteriorates. In such case, he may not claim any reimbursement of expenses incurred.

Extension of services: help to organise return journeys.

Beneficiaries whose medical condition does not justify repatriation for medical reasons, under the conditions hereby described, may nevertheless benefit from logistical help (without costs being incurred by MUTUAIDE) for the organisation of their transfer between the place where they retired on the Rally and Santiago (or Lima).

a) Transfer and/or repatriation of the Beneficiary

If the Beneficiary's state of health leads, under the terms advised above, the Medical Director of MUTUAIDE, in agreement with the Rally medical team, to decide to transfer or repatriate the Beneficiary, MUTUAIDE will take care of transportation.



Transportation will be by whatever means is appropriate (ambulance, scheduled flight, air ambulance, etc.) under medical supervision if necessary. Only the medical interests of the Beneficiary and compliance with the health regulations in force shall be considered in selecting the means of transport.

IMPORTANT: This service will never be used in the case of minor lesions which can be treated locally and which do not prevent the Beneficiary from continuing in the Rally or from getting to Santiago by their own means (with the exception, for certain cases, upon the Medical Director of the Rally's decision, of the medical costs").

Nota: The obligation to stop the Rally will not systematically give the right to repatriation costs being covered.

CAUTION: If a Beneficiary resident or domiciled outside Europe (or one of the three host countries of the Rally) is repatriated from Europe to his country of domicile or residence, this shall be at his own expense and it is strongly recommended that transport be arranged and/or specific insurance taken out and to check with the insurer exactly what is covered.

b) Medical expenses (including the cost of hospitalisation) incurred during Dakar 2013

With the prior agreement of its Medical Director, MUTUAIDE will pay, up to a limit of **€4,500 including tax per Beneficiary**, medical fees, the cost of medicines prescribed by a doctor or surgeon, the costs of hospitalisation if decided by the medical team.

Medical costs incurred in the Beneficiary's country of domicile must be paid by the Beneficiary.

For Beneficiaries from Peru, Argentina or Chile, medical expenses will cease to be paid from the moment of repatriation either to the domicile, or to a medical institution close to the domicile.

The Beneficiary undertakes to apply to all health insurance companies through which he has cover with a view to obtaining reimbursement of all or part of his medical and hospitalisation costs and to repay any amounts to MUTUAIDE.

IMPORTANT: Medical costs exceeding €4,500 (and costs incurred in the country of domicile) to be paid by the Beneficiary; it is strongly recommended that specific medical insurance be arranged and that a check is made with the insurer regarding what is covered, in particular given participation in a competition.

This "Medical Expenses" cover, (which cover medical expenses and hospital costs resulting from an accident during the Rally and incurred outside of the country of residence of the entrant) may be arranged with the Sport Events Department of Gras Savoye whose contact details, offer and website address can be found on the Rally website at www.dakar.com.

The taking out of 'medical expenses' cover is optional but strongly recommended; it can be linked with an Individual Accident Insurance which is also strongly recommended. So as to facilitate the use of any insurance cover, the details of the insurer will be taken by the medical team during administrative checks.

Extended service: advance of hospitalisation costs

If a Beneficiary is unable to meet the cost of medical expenses in excess of €4,500, MUTUAIDE may agree to advance funds.

This will be done against a guarantee cheque made out to MUTUAIDE ASSISTANCE or an IOU signed by the Beneficiary or his legal representative.

In all cases, any sums advanced are repayable within thirty (30) days of the funds being made available.

If payment is not received, MUTUAIDE reserves the right to take any action required to recover the monies from the Beneficiary.

c) Repatriation of corpse

If a Beneficiary dies during the DAKAR 2013, MUTUAIDE will pay for:

- the cost of transporting the corpse to the funeral parlour closest to their place of domicile;
- the costs associated with preserving the corpse in compliance with legislation;
- costs directly associated with transporting the corpse.

The Beneficiary's family is responsible for all other costs.

MUTUAIDE alone is responsible for selecting the companies to be involved in the repatriation process.

If the local authorities expressly demand that an eligible person is present in order to implement the necessary repatriation steps, MUTUAIDE will organise and pay for such person's return journey as well as accommodation costs up to €500 including tax.

d) Visit by a relative

If the Beneficiary's state of health requires hospitalisation for a period greater than five (5) days prior to repatriation, MUTUAIDE will organise and pay for a return journey for a person designated by the Beneficiary and domiciled in the same country as the Beneficiary to visit the Beneficiary's bedside.

MUTUAIDE will also organise and pay for this person's accommodation up to €100 per night, including tax, up to the date of repatriation.

The maximum total cover for a visit by a relative (transport + accommodation) is €4,000 including tax.

This person must pay their own subsistence costs.

e) Expected return

If a Beneficiary is forced to interrupt his participation in the Rally as a consequence of the unexpected hospitalisation for a minimum of five (5) days, or the death, of a member of his family, MUTUAIDE will organise and pay for his return to his country of domicile.

f) Legal assistance

- If, outside his country of domicile, a Beneficiary is subject to legal proceedings or is incarcerated for unintentional failure to comply with, or breach of, local legislation and regulations: MUTUAIDE will advance any bail demanded by the local authorities to secure temporary release of the beneficiary, up to a limit of €15,000 including tax.

This will be done against a guarantee cheque made out to MUTUAIDE ASSISTANCE or an IOU signed by the Beneficiary or his legal representative.

In all cases, any sums advanced are repayable within thirty (30) days of the funds being made available.

If payment is not received, MUTUAIDE reserves the right to take any action required to recover the monies, from the Beneficiary.

- MUTUAIDE may also help a beneficiary to appoint a legal adviser and to pay their fees up to a limit of **€4,000 including tax**.

g) Replacement driver

If, following a physical accident or illness affecting the Beneficiary, none of the other Beneficiaries (co-driver, assistance) is able to drive in his place, MUTUAIDE will provide and pay for a replacement driver to take the Vehicle to Santiago or to the port of San Antonio, by the most direct route, provided the Vehicle is in a state which complies with technical control standards.

The Beneficiary is responsible for the costs associated with returning the Vehicle (tolls, fuel) as well as any hotel and subsistence costs.

h) Psychological support

If, following an incident involving physical injury, the Beneficiary suffers psychological trauma, he may receive emergency psychological assistance.

To guarantee this service, they must contact MUTUAIDE within a maximum of thirty (30) days following the traumatic event and provide their GP's contact details.

On receipt of any such request, MUTUAIDE will, provided the Beneficiary is in a suitable state of health, and on the advice of the MUTUAIDE doctor, organise emergency psychological assistance.

This support will be provided by a psychologist and includes the organisation and payment of three telephone consultations.

In all cases, the decision to provide emergency psychological support will be taken exclusively by the MUTUAIDE Medical Director, possibly after contacting, and with the agreement of, the beneficiary's GP.

This service is available in the following languages: French, English, Spanish.

3) DISQUALIFICATIONS

a) No assistance/repatriation hereby detailed will be provided for conditions or minor lesions which may be treated in situ, or for conditions which do not stop the Beneficiary from continuing the Rally or returning to Santiago by their own means.

(With the exception of certain cases of the 'Medical Expenses' cover, to be decided upon by the Medical Director of the Rally).

b) No assistance will be provided in the case of physical attack or death resulting:

- from an intentional or deceitful act on the part of the Beneficiary;
- from involvement in wagers, fights, brawls;
- from non-emergency pathological conditions;
- from nervous illness, nervous depression, mental health conditions;
- from use by the beneficiary of medications, drugs, narcotics, tranquillisers and/or similar products which are not medically subscribed;
- from a state of inebriation characterised by the presence of a level of pure alcohol in the blood which is equal or superior to that which applies to legislation governing driving a car in France, and in force on the date of the accident;
- from suicide or attempted suicide.

c) The following costs will not be met under any circumstances:

- the cost of medical equipment, braces and prosthetics;
- any spa treatments of any kind;
- aesthetic treatments;
- the cost of rehabilitation, physiotherapy, chiropractic;
- the cost of purchasing vaccinations and the costs of vaccination;
- the cost of medical or paramedical services and the cost of purchasing therapeutic products not recognised under French law;
- the cost of a final coffin;
- customs costs.

Participants will be provided with a document about the services described above during the administrative checks.

33P2 PUBLIC LIABILITY

The Organiser has arranged a Public Liability Insurance policy with AXA France which relates specifically to sports events with terrestrial vehicles in accordance with the legislation in force.

The rights of engagement under the insurance premium cover the Competitor's public liability towards third parties up to the following amounts per event:

- €2,500,000 for material damages;
- €13,100,000 for physical damage;
- or the minimum in vigour in the countries crossed

The purpose of said contract is thus to cover, in the case of an accident, fire or explosion during the course of the Rally, financial consequences arising from civil liability which may be the responsibility:

- of Competitors as a result of physical or material damage caused to third parties to agents of the State or any other public body involved in the order, Organisation or control of the Rally;
- of Competitors as a result of physical or material damage caused to other Competitors during Road Sections but in Selective Sections, uniquely if the responsibility of one of them may clearly and without ambiguity be determined, admitted and proved;
- of Drivers in regard to their team mates and passengers resulting from accidents incurred on public roads not reserved for private use (notably during Road Sections) or during Selective Sections on condition that the responsibility of the Driver can be proved.

The insurance contracts which the Organiser has arranged in no cases covers (non-exhaustive list):

- accidents caused by strike, riots, crowd movements, civil or foreign war;
- the responsibility of an insured party in the case of an accident caused by intention or deceit;
- the responsibility of an insured person in the case of damage to assets which he owns, has rented, is the depository or guardian of;
- fines;
- the theft of vehicles, separate items and all other property (the Organiser accepts no liability, therefore, in the case of theft which occurs in a country being crossed by the Rally).

Period of cover:

for Vehicles embarking on ferries organised by the Organiser insurance cover will come into effect at the moment of Vehicles entering the Locked Park at the port of Le Havre (November 2012) and will expire on exit of Vehicles from the Locked Park at the port of Le Havre (at the latest on 28th February 2013 at 11.59pm). You are reminded that the sea crossing is not covered by this policy.

- for other Vehicles (not transported in ferries organised by the Organiser) : insurance cover will take effect at the moment Vehicles enter scrutineering in Lima (Peru), therefore from 2nd January 2013, and will end after the finish podium in Santiago (Chili), thus at the latest the 20th January 2013 at 11.59pm.

Vehicles circulating in Peru, in Argentina and in Chile outside of the periods covered by the guarantee must take out local insurance.

Caution: the vehicle insurance in Peru is different from the one in Argentina and Chili (Mercosur). All the Vehicles which do not embark on board of the ferry set up by the Organiser must therefore contract an insurance in each of the 3 countries crossed.

Caution: this policy covers the third party insurance for the vehicles during their participation in the Rally, within the official route (Selective Sections and Road Sections) of the Rally indicated by the Organiser. The Competitors who move away from it (notably before administrative checks and scrutineering or following their withdrawal) must contract an insurance for their vehicle depending on the local Regulations.

Vehicles disqualified or having withdrawn will still be covered only upon the condition they rejoin Santiago or the port of San Antonio Valparaiso by the most direct route from the place they withdrew or by the Event's itinerary.

In the case of accident, the Competitor or his representative must make a written statement, within 24 hours, to the Clerk of the Race, and the Head of Competitor Relations (or possibly directly to the Organiser's headquarters). The report must mention the circumstances of the accident and contact details of any witnesses.

Competitors thus enter the Dakar Rally 2013 "Perú – Argentina – Chile" fully aware of the risks this Rally could lead them to take.

33P3 PERSONAL ACCIDENT INSURANCE

Competitors must contact their National Sporting Authority to find out what their sport driving licence covers them for. **It is also strongly advised that they take out additional insurance with the insurance company of their choice.**

This Personal Accident Insurance (which guarantees the payment of lump sum in the case of death or permanent injury following an accident during the course of the Rally) may be arranged with the Sport Events Department of Gras Savoye whose contact details, offer and website address can be found on the rally website at www.dakar.com.

This offer can be supplemented with optional "Medical Expenses" cover (which will reimburse medical expenses incurred outside the country of domicile as the result of an accident during the course of the Rally).



This Personal Accident insurance, as well as the “Medical Expenses” cover is optional, but is strongly recommended.

As indicated above, the medical expenses entered into, with the agreement of the Medical Director, will be covered by MUTUAIDE up to the limit of €4,500.

It may be arranged by contacting the Sport Events Department of Gras Savoye prior to the Rally's departure, or, at the latest, during checks at Mar del Plata. A cover note and information sheet will also be sent afterwards.

Gras Savoye is at the disposition of participants for all other requests presented before 16th December 2012.

The complete text of the insurance contract is available upon request from Gras Savoye.

34P TIME CARD

- 1) At the start of a Stage, Crews shall be given a Time Card on which the target times and the maximum times authorised to cover each Road Section and each Selective Section shall appear respectively. This Time Card is handed in at the Finish Time Control of each Stage and replaced by a new one at the start of the next Stage. Each Crew is solely responsible for its Time Card.
- 2) Any correction or amendment made to the Time Card will result in Disqualification, unless such correction or amendment has been approved in writing by the controller.
- 3) The Crew alone is responsible for submitting the Time Card at the different controls and for the accuracy of the entries. The controllers are the only persons allowed to enter the time on the Time Card, by hand or by means of a print-out.
- 4) Crews are obliged, under pain of penalties which may go as far as Disqualification, to have their passage checked at all points mentioned on their Time Card, and in the correct order. The absence of a stamp or the failure to hand in the Time Card at any control will result in a penalty which may go as far as Disqualification.
- 5) At all the various controls, the Time Card may only be presented if the Crew is complete and aboard the Vehicle, on pain of Disqualification.
- 6) **The loss of a Time Card will entail a 5-minute penalty.**

35P CONTROL ZONES

- 1) All controls, i.e. Time Controls, Start and Finish of Selective Sections, Passage Control will be indicated by means of a standardised double sign.
- 2) The beginning of the control area is marked by 2 warning signs on a yellow background. At a distance of about 100 m, the position of the control post is indicated by 2 signs on a red background. The end of the control area, approximately 100 m further on, is indicated by 2 final signs on a beige background with three black transversal stripes.
- 3) The Time Control Zone should be organised in such a way that Team members can watch the timekeeping without entering the Control Zone. The maximum width of the zone may not exceed 15 m and the zone must be situated along a single axis, except for Passage Control Zones.
- 4) It is strictly forbidden to enter and/or leave a control area from any direction other than that prescribed by the Road Book and/or to re-enter a control area once checking-in has taken place at this control, on pain of:
 - 1st infringement: penalty of 10 minutes,
 - 1st repeated offence: penalty of 1 hour,
 - 2nd repeated offence: Disqualification or Fixed Penalty where this exists.
- 5) The official time is the GPS time.

The target check-in time is the responsibility of the Crews alone, who may consult the official clock on the control table.

The Rally time (timings, Briefings, etc.) will be GMT-5 in Peru and GMT-3 in Argentina and Chile. The evening of the Arequipa-Arica Leg at 01.00am it will therefore be 03.00am.

6) Control posts shall be ready to function 30 minutes before the target time for the passage of the first crew. Unless the Clerk of the Race decides otherwise, they will cease to operate 1 hour after the target time for the last Crew.

The Time Controls for the Start and Finish of Selective Sections will cease to operate at a time calculated taking into account the maximum time(s) allowed preceding this control for the last classified Competitor.

7) On pain of a penalty which may go as far as Disqualification, the Crews are obliged to follow the instructions of the marshal in charge of the control post, who must be assisted by a controller (all possible cases will be examined by the Sporting Stewards following a written report by the marshal in charge of the control post).

8) A Vehicle must move by its own means. If the Vehicle is unable to move, towing and/or pushing by a Competitor in the race is allowed, or by an assistance Crew or Vehicle when the race and the assistance route follow the same itinerary.

36P TIME CONTROLS

36P1 GENERAL

1) At the Time Controls, the controllers will indicate on the Time Card the check-in time, which corresponds to the exact moment at which one of the Crew members submits the Time Card to the controller. The clocking of the Time Card will only be carried out if all the Crew members and the Vehicle are within the immediate vicinity of the control table.

2) The check-in procedure commences the moment the Vehicle passes the entry sign for the Time Control Zone.

3) The Crew is forbidden to stop or to drive abnormally slowly between the zone entry sign and the control post. A Crew may remain in the Control Zone only for as long as is necessary to complete the check-in formalities, scrutineering and GPS checks.

4) The Crew does not incur any penalty for early arrival if the Vehicle enters the Control Zone during the minute before the target check-in minute.

5) For Road Sections, the target check-in time is that obtained by adding the time allowed for completing the Road Section to the starting time for that Section. These times are expressed in hours and minutes and are always shown from 00.01 to 24.00.

6) For Road Sections, the crew does not incur any penalty for late arrival if the time card is submitted to the controller during the target check-in minute. For example, a crew which is supposed to check in at a control at 18h 58 min. shall be considered on time if the check-in takes place between 18h 58 min. 00 sec. and 18h 58 min. 59 sec.

7) At the Time Control at the Finish of a Road Section, the controller will enter on the Time Card on the one hand the check-in time of the crew and, on the other, its provisional starting time for the Selective Section. There must be a 5-minute gap between the two to allow the crew to prepare for the start.

a) Immediately after checking-in at the Time Control, the Crew will go to the Start of the Selective Section. The controller will enter the time foreseen for the Start of the Selective Section and will then give the start for that crew.



b) In the case of an incident, should there exist a divergence between the two entries, the starting time of the Selective Section will be binding, unless the Sporting Stewards decide otherwise.

8) At a Time Control, any difference between the target check-in time and the actual check-in time will be penalised at a rate of one minute per minute or fraction of minute.

9) For Selective Sections, the finish times will be taken at the maximum to the second.

10) At the Time Control at the end of a Stage, and at the Parc Fermes, Crews are authorised to check-in ahead of time without incurring a penalty.

11) Any failure on the part of a Crew to observe the rules of the check-in procedure defined above (and in particular the fact of entering a Control Zone more than a minute earlier than the check-in time) will be recorded by the chief controller at that post and sent in a written report to the Clerk of the Race.

12) The Disqualification time, or one or more maximum times may be modified at any moment by the Sporting Stewards, upon the proposal of the Clerk of the Race. The Crews concerned shall be informed of this decision as soon as possible. Disqualification from the Event or the Fixed Penalty for exceeding the maximum time allowed may only be announced at the end of a Leg.

13) When a Time Control is followed by a Start of a Selective Section, these two posts shall be included in a single Control Zone, the signs of which shall be laid out as follows:

- 2 yellow warning signs with clock (beginning of zone);
- 2 red signs with clock (Time Control post) after approximately 100 m;
- 2 red signs with flag (Start of the Selective Section) at a distance of 50 to 200 m;
- finally, 2 end of control signs (3 transversal black stripes on a beige background) 100 m further on.

14) If the Start of a Selective Section coincides with the start of a Stage, the starting time of the Selective Section will also be that of the Stage.

15) At the Finish of a Selective Section, the stop point will be twinned with a Time Control.

16) If a Time Control is not followed by the Start of a Selective Section, the checking-in time on the Time Card constitutes both the arrival time at this Time Control and the start time for the new Section.

36P2 LATE AT THE TIME CONTROL BEFORE THE START OF A STAGE

All checking-in late at the start of a Stage will be penalised by a minute per minute late, until the closing of the control (i.e. one hour after the ideal start time of the last Competitor).

A new time and a new start order will be given to the Crew, at the discretion of the person in charge of the control.

36P3 LATE AT TIME CONTROL BEFORE THE START OF A SELECTIVE SECTION

All checking-in late at the Start of a Selective Section will be penalised by a minute per minute, until the closing of the control (i.e. one hour after the ideal start time of the last Competitor).

A new time and a new start order will be given to the Crew, at the discretion of the person in charge of the control.

36P4 TARGET TIME

1) Road Section

A target time is given for each Road Section.

Any difference in this target time will incur a penalty of one minute per minute.

Clocking in ahead of time is authorised at the Time Control at the Finish of a Road Section at the entrance of the Bivouac.

2) Selective Sections

Selective Sections will be run in real time, with a maximum time allowed. Crews checking-in after this maximum time will incur the Fixed Penalty for the day. If the Finish of the Selective Section is also the finish of the Stage, the procedures laid down in Article 36P5 will apply. The arrival time will be taken no more than to the second.

3) Timing (itinerary/schedule)

Target times and maximum times allowed, the start order and the gaps will be given at administrative checks, as will the closing Times of Controls (CP and TC) and the Fixed Penalties.

36P5 OVERRUNNING OF THE MAXIMUM TIME ALLOWED AT THE TC AT THE END OF THE STAGE

1) All Competitors arriving at a TC at the Finish of a Stage after the maximum time allowed will be penalised by a minute for every minute late or the maximum time + the Fixed Penalty on Stages where clocking-in, in real time, is not possible.

After the closure of the control, clocking in must be done at the Race Control (PCO) or at the TC Start of the Stage, with the Official present.

2) All Vehicles arriving at the end of a Stage after the closing of the Time Control may take the start for the following Stage under the following conditions:

- they present themselves at the Time Control at the Start of the Stage before the closing of the control;
- they get their Time Card from the preceding Stage stamped and then hand it to the Clerk of the Race or the Official in charge of the Start Time Control.

A new start time and start order will be given to the Competitor, at the discretion of the steward in charge.

3) All Vehicles that cannot manage to retake the start of a Stage under these conditions will be disqualified: they may continue the Event in the "assistance" category (Article 31P6).

36P6 OVERRUNNING THE MAXIMUM TIME ALLOWED ON THE REST DAY

For the rest day, the latest check-in time, so as to be able to start the following day, is fixed at 06.00pm. After this time, Vehicles arriving from the previous day's Stage will be considered as being disqualified.

36P7 INTERVIEWS

1) Finish of Selective Section

Areas especially dedicated to interviews will be settled each day after the Finish of the Selective Section. The selected Competitors are required to stop there the time imposed by the Organiser.

A new start time will be given to the Competitor when leaving the interview zone.

Failure to respect these Regulations, or any incivility noted, will result in a penalty of €500. Penalties for repeated infractions will be decided upon by the College of Sporting Stewards.

2) Finish of Road Section

The selected Competitors will have to go to the interview zone, located close to the Media Center, after their arrival at the Bivouac.

Failure to respect these Regulations, or any incivility noted, will result in a penalty of €500. Penalties for repeated infractions will be decided upon by the College of Sporting Stewards.

37P PASSAGE CONTROLS

37P1 GENERAL

1) In order to check that the Crews are respecting the itinerary in the Road Book, Passage Controls will be set up at a significant location mentioned and numbered in the Road Book. They will be noted on the time cards.

- 2) The Control Zone will be defined using the following official double signposting:
 - a) 2 yellow signs with stamp (start of zone);
 - b) after approximately 100 m, 2 red signs with stamp (Passage Control post);
 - c) Finally, 100 m further on, 2 final beige signs with 3 transversal black stripes.
- 3) The location of these Passage Controls must be clearly visible and signalled to Crews by means of flags and, wherever possible, sited on fairly level hard ground, otherwise, the ground must be downhill.
- 4) The Vehicle passage will be timed to the second and noted on a passage sheet by the person in charge of the post.

37P2 CLOSING TIME FOR PASSAGE CONTROLS

- 1) The closing time for Passage Controls will be declared taking into account:
 - the distance covered since the start of the Section concerned,
 - the time average of the Section under consideration (Selective or Time) imposed by the maximum allowed time,
 - the ideal time of the last Vehicle, increased by 60 minutes.
- 2) This time will be mentioned on the timing (itinerary/schedule) given during administrative checks.

37P3 CLOSING OF PASSAGE CONTROLS

After the closure of a Passage Control, the validation by the GPS of the corresponding Way Point will be taken into account and will attest to the respect of the official itinerary by the Crew. In this case, there will be no penalty for missed CP.

38P SELECTIVE SECTIONS

38P1 GENERAL

- 1) During the Selective Sections, all members of the Crew must conform to the standard described in the FIA Appendix L, Chapter 3, under pain of Disqualification. Checks may be made by Officials at the start of the Selective Sections. In case of any infringement to Appendix L, the start will be refused to the concerned crew.
- 2) Crews are forbidden to drive in the opposite direction to that of the Selective Section, under pain of penalties which may go as far as Disqualification.
- 3) At the starts of Selective Sections, when the Vehicle with its Crew on board has stopped in front of the starting control, the controller will enter the actual time of the start of the Vehicle in question on the Time Card (hour, minute and second), and will then count down aloud: 30" - 15" - 10" and the last 5 seconds one by one. When the last 5 seconds have elapsed, the starting signal shall be given, upon which the Vehicle must start immediately. A 2-minute penalty shall be imposed on any crew which fails to start within 20 seconds of the starting signal. If a Competitor is unable to leave the control zone under his own power, the penalties as set out must be applied.
- 4) The start of a Selective Section may only be delayed in relation to the scheduled starting time by the controller in a case of "force majeure".
- 5) A false start, particularly one made before the controller has given the signal, shall be penalised by 1 minute at least or an increased time applied by the Sporting Stewards based on the controller's report. This penalty does not exclude heavier penalties being inflicted by the Stewards, especially if the offence is repeated.
- 6) Selective Sections will end in a flying finish, the official double signposting as follows:
 - a) 2 yellow chequered signs (beginning of zone);
 - b) After approximately 100 m, 2 red chequered signs (flying finish);

- c) At a distance of 150 to 300 m, 2 x 2 red signs (clock and STOP);
- d) Finally, approximately, 100 m further on, 2 final beige signs with 3 transversal black stripes.

7) Stopping between the yellow warning sign and the STOP sign is forbidden; any infringement will entail a 15-minute time penalty. Timekeeping will be done on the finish line, which must have print-out equipment and shall be backed up by hand timing.

8) At a distance of 150 to 300 m after the finish, the crew must report to a control indicated by a red clock and a red STOP sign. The controller will enter on the Time Card the time of arrival (hour, minute and seconds), which will also be the starting time of the following Road Section (hour and minute). If several Competitors arrive during the same minute, the marshal in charge of the post will stagger the starting times of these Competitors at intervals of at least 30" in the order in which they arrived.

9) If a Competitor does not stop at the STOP point to have their times entered, a 1-hour penalty shall be applied.

10) During a Selective Section, unless in the case described in article 31P2.2.a any assistance is forbidden other than that of a racing crew using parts transported by another racing crew. However, approved assistance zones may be set up by the Organisers.

11) The starting intervals for Selective Sections must respect the same dispositions as those laid down for starting Stages except in a case of a road penalty.

12) Any Crew refusing to start in a Selective Section at the time and in the position allocated to it shall be given a penalty of 15 minutes and must make itself available to the post chief who will notify it of its new start time.

38P2 INTERRUPTION OF A SELECTIVE SECTION.

When a Selective Section is definitely stopped before the passage of the last crew, regardless of the reason, a classification for that section may nevertheless be obtained by attributing a time to all those Crews who were affected by the circumstances of the interruption.

These times may be calculated in the following manner:

1) Interruption of a Selective Section at a given point. Taking of time at a Way Point during a Selective Section:

a) Given that a time is taken at each Way Point and recorded in the GPS Unik, a result for a Selective Section may be obtained using the time taken at the Way Point preceding the point of interruption.

The times at the various Way Points recorded in the GPS will be transmitted by radio when Competitors arrive at the Time Control at the Bivouac.

b) In the case of a malfunctioning GPS, a Competitor's time will be calculated using their position on the track at the time of interruption (Iritrack position) and the times of the Competitors who have past just before or after them at this point. The Sporting Stewards can choose as the time of reference that which seems the most appropriate.

c) In the case of a malfunctioning GPS and Iritrack, the sporting stewards will undertake the necessary research concerning the position of the Vehicle so as to establish an appropriate time.

2) Interruption of a Selective Section at several distinct points. Taking of time at the Time Control at the end of the Selective Section:

a) In this case a time will be attributed to all the Crews concerned by the interruption. This time will be calculated by taking the worst time of the Crew having crossed the finish line, to which will be added the Fixed Penalty and 5 minutes per Way Point missed (DZ, FZ, WPM, WPV,



WPE, WPS, CP), between the point at which the Crew was stopped and the finish of the Selective Section.

b) Should the Sporting Stewards consider the worst time actually set to be abnormal, they may choose as a reference time the one which they feel to be the most suitable.

3) It is up to the Sporting Stewards alone to decide whether or not to apply this measure, once Race Control has informed them of the reasons for the interruption.

4) Any Crew which is responsible or partly responsible for the stopping of the race cannot, under any circumstances, benefit from this measure. Any such Crew will therefore be credited with the actual time which it may have set, if this is greater than the imaginary time attributed to other Crews.

5) In exceptional cases, for safety reason, the Clerk of the Race may interrupt the Selective Section at a given point and the Competitors may continue the Selective Section once the dangerous zone has been passed. The results will be established by adding together the times of the two portions of this Selective Section.

38P3 SAFETY

Selective Sections are run on sections of 'road' open to the public. The greatest care is recommended in relation to other eventual users.

38P4 NEUTRALISATION

On several Stages a neutralisation will be effected in the form of a transfer, with a target time, to separate two sections of the same Selective Section.

The Crew will hand in its Time Card to the Time Control at the beginning of the Neutralisation, on which will be noted the time at which they start the Neutralisation (finish time of the 1st section) and their time at the end of the Neutralisation which will also be their start time for the 2nd section.

During these Neutralisations the maximum speed allowed, checked by GPS, will be that of the speed limit of the country being crossed.

All assistance is forbidden on the itinerary of the Neutralisation.

39P REGROUPINGS

1) The purpose of regroupings is to reduce the intervals which may occur between Crews as a result of late arrivals and/or withdrawals. The Parc Ferme conditions apply.

2) On their arrival at these regroupings, the Crews will hand their Time Card to the controller. They will receive instructions on their starting time.

They must then drive their Vehicle immediately and directly to the Parc Ferme. The starting order shall be that of the arrival at the regrouping Time Control. The distance between the TC and the Parc Ferme will be considered as a Parc Ferme area.

3) For safety reasons, on the proposal of the Clerk of the Race, the Sporting Stewards may decide to stop a Stage before its allotted finish. A Time Control will then be set up, acting as the end-of-Stage control (idem: for a Selective Section, the Time Control being twinned with the end-of-Selective Section control), where the times recorded will serve to establish the classification of the Stage.

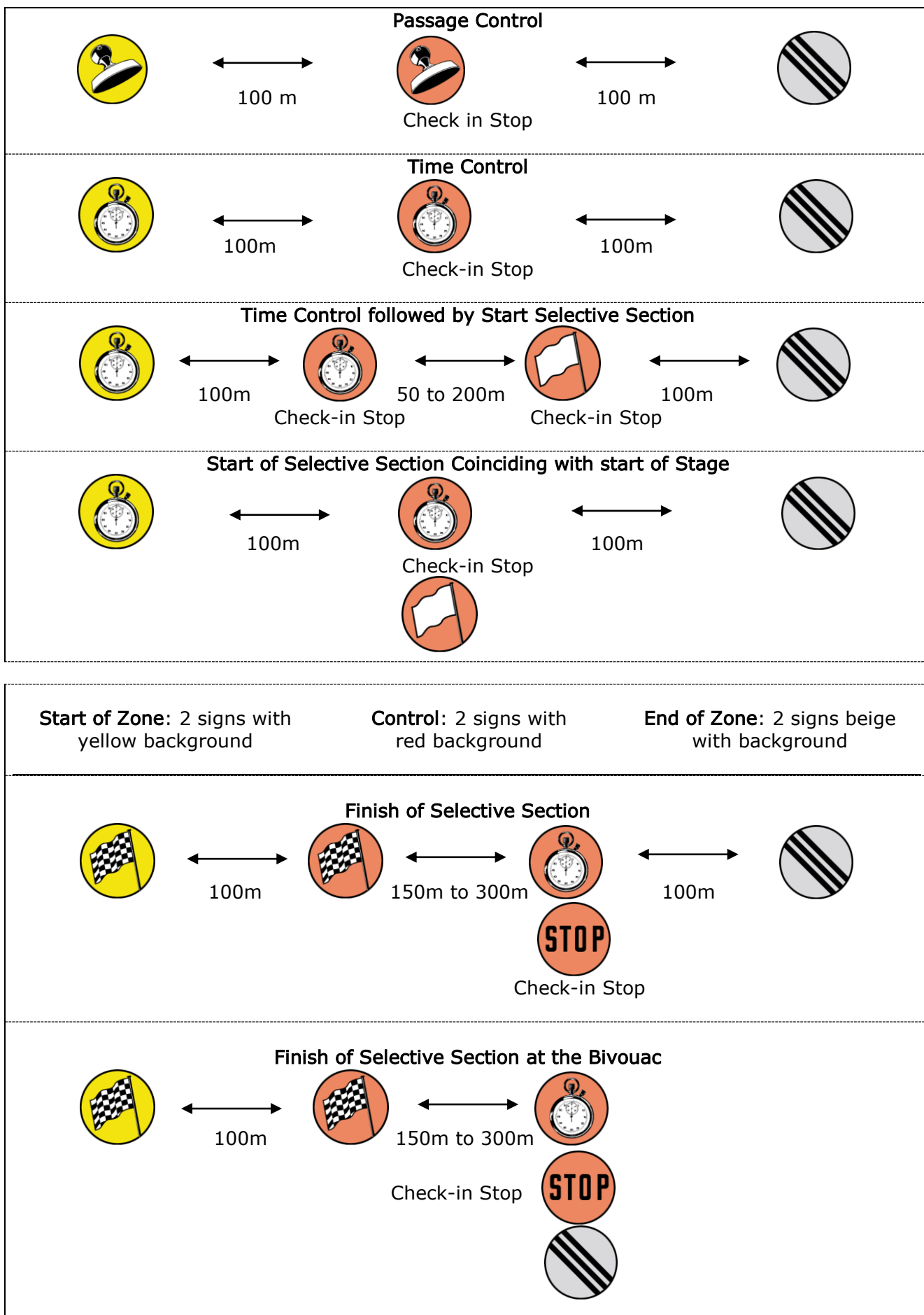
On the decision of the Clerk of the Race, this Stage may or may not be continued, neutralised or in convoy, and may or may not be under the Parc Ferme rules.

40P SIGNPOSTING OF CONTROLS

Start of Zone: 2 signs with yellow background

Control: 2 signs with red background

End of Zone: 2 signs beige with background



Finish of Road Section at the Bivouac



Check-in Stop



41P1 GENERAL

1) The following rules will apply:

- a) It is forbidden to refuel or repair the car in a Parc Ferme.
- b) Starting the car with the help of towing or pushing from another Competitor still racing shall be penalised by 1 minute.
- c) Vehicles will be in Parc Ferme from the moment they enter a Parc Ferme for the start, regrouping or end of Stage, until they leave it.
- d) Vehicles will be in Parc Ferme from the moment they enter a control zone. From the Stop point until the zone exit, if the Vehicle is unable to restart, it may only be either towed or pushed out of the zone with external help without penalties, under the supervision of the chief marshal.
- e) Vehicles will be in Parc Ferme from as soon as they reach the end of the last Stage (and at least until the time for lodging protests has expired).
- f) Vehicles will be in Parc Ferme where scrutineering is carried out at the finish of the Selective Section or of the Stage.

2) Except in the case of the finish - Bivouac section Time Control, any infringement of the Parc Ferme Regulations shall result in a penalty ranging from 10 hours to Disqualification.

3) Before the exit from all the parks or at the start of a Stage, if the Scrutineers of the Event note that a Vehicle seems to be in a condition which is not compatible with normal use, they must immediately inform the Clerk of the Race, who may request that it be repaired.

In this case, the time in whole minutes used for such repairs shall be considered as the same number of minutes lateness registered on a Road Section. To prevent a Crew from trying to make up lost time after repairs, the Crew shall be given a new starting time. Any Crew which arrives more than 30 minutes late will be disqualified.

4) After having parked their Vehicle in the Parc Ferme and in case of bad weather, the Crews will be authorised to cover their Parc Ferme with transparent covers. Crews must leave the Parc Ferme immediately and no member of the Crew will be allowed to re-enter it.

5) By way of exception to the Parc Ferme rules, and on the responsibility of an Official, the Crew may, while in the Parc Ferme at the start, regrouping zone or end of Stage:

- change complete wheels, one or two punctured or damaged tyres using the equipment on board;
- have a new windscreen fitted with the possibility of having outside help with the agreement of the Clerk of the Race;
- check and/or adjust the pressure of their tyres;
- Have installed, adjusted or removed an on board camera, by the Organisers' agreed supplier.

These repairs must be completed before the starting time, otherwise a penalty of one minute for every minute's lateness shall be imposed.

6) In order to remove its Vehicle from a Parc Ferme for the start, regrouping halt or end of Stage, the Crew shall be allowed to enter the Parc Ferme 15 minutes before their starting time, except in Lima where they will be allowed to enter the parc 30 minutes before their starting time.

7) Inside the Parc Ferme, the engine may be started by means of an external battery, which may be brought in and taken out under the supervision of an Official of the Event. Under no circumstances may this battery be taken on board the Vehicle (except in the cargo body bodywork of a T4 Vehicle).

8) Before leaving the Parc Ferme, the Crews are permitted to place their Vehicle covers and/or external battery outside the Parc Ferme.

41P2 SPECIFICS

- 1) Vehicles will be subject to the Parc Ferme rules:
 - a) On the Port of Le Havre, before loading the Vehicles and during the crossing;
 - b) After unloading the Vehicles in Lima, on the Port of Callao, up to 31st December 2012 and 1st January 2013;
 - c) On the administrative checks and scrutineering site in Lima, on coming out of scrutineering on 2nd, 3rd and 4th January 2013;
 - d) On 19th January 2013 after La Serena-Santiago Stage in the Parque O'Higgins in Santiago;
 - e) After the podium, on 20th January 2013 in Santiago. Parc FermeThe Parc Ferme will become a guarded park, up to Monday 21st January 2013 at 08.00am, when it will have to be empty.

42P ADMINISTRATIVE CHECKS AND SCRUTINEERING

42P1 GENERAL

- 1) All Crews must report to the administrative checks with all their members. Financial penalties for lateness at the preliminary scrutineering, according to the timetable laid down by the Organiser, may be applied by the Sporting Stewards.
The Team with its official representative must present the Vehicle or Vehicles and all necessary documents at scrutineering.
If scrutineering is followed by a Parc Ferme, a member of the Team may take the car(s) into that parc. The check-in times, if any, must be respected, and it will be the responsibility of the Team to ensure that they are.
Any Crew reporting to the scrutineering area and/or administrative checks outside the time limits prescribed for in the Regulations will not be allowed to start, except in a case of force majeure duly recognised as such by the Sporting Stewards.
- 2) Only those Crews who have passed the administrative checks may present themselves with their Vehicle equipped with its rally plates and rally numbers at scrutineering, which will be of a completely general nature: brand and model of the Vehicle, apparent conformity with the group in which it is entered, conformity of the safety items, conformity of the Vehicle with the Highway Code. After scrutineering, if a Vehicle is found not to comply with the technical and/or safety Regulations, the Sporting Stewards may set a deadline before which the car must be made to comply.
- 3) No Vehicle will be allowed to start unless it complies with the FIA safety Regulations, the present Regulations and its appendices.
- 4) In conformity with article 145 of the Code, additional checking of the Crew members as well as of the Vehicle may be carried out at any time during the Rally.
At all times during the Rally, the Competitor is responsible for the technical conformity of his Vehicle. The fact of presenting a Vehicle for scrutineering is considered as an implicit statement of conformity.
- 5) Should new identification marks be affixed, it is the responsibility of the Crew alone to see that these are protected until the end of the Event, their absence leading to Disqualification.
- 6) Thorough scrutineering involving the dismantling of the Vehicle for the Crews in the first places of the general classification and in each group and category, and possibly for any other crew, may be carried out at the absolute discretion of the Sporting Stewards ex officio or following a protest or upon the decision of the Clerk of the Race.

42P2 ADMINISTRATIVE CHECKS AND SCRUTINEERING IN LIMA

- 1) The Competitors will pass the scrutineering and administrative checks in Lima on 2nd, 3rd and 4th January 2013.
On 1st January 2013 from 2.00pm, South American Competitors will be able to collect their safety equipment and will be called to administrative checks in priority on 2nd January 2013.

2) Competitors will receive a convocation stating the day and exact time at which they must present themselves at the administrative checks.

Failure to respect the convocation times will incur a penalty of:

- €50 for the first hour late,
- €80 for the following hours.

A Time Control will be put in place at the entrance of the Waiting Park of the administrative checks.

3) On leaving administrative checks, Competitors will receive a convocation time for scrutineering. All lateness for scrutineering will be sanctioned as follows:

- up to 30 minutes: €15;
- from 30 minutes to 1 hour: €30;
- per hour extra: €15.

4) On coming out of scrutineering all Vehicles will be placed in Parc Ferme.

5) During administrative checks, Competitors will be given a compulsory course on Safety Instruments: Sentinel and Iritrack. All absence will incur a penalty of €500.

6) Vehicles must be presented in Le Havre, for boarding, with the various mounting brackets, cables and aerials of the different safety equipment already fitted, ready to receive these systems (GPS, Iritrack), on pain of being refused embarkation.

7) Vehicles being presented directly in Lima, must present themselves at scrutineering with the various mounting brackets, cables and aerials of the different safety equipment already fitted, ready to receive these systems (GPS, Iritrack). Failure to do so will result in a penalty of €150 per item of equipment not installed.

8) The Iritrack and GPS must be connected directly to a battery so that they continue to function when the engine is stopped. If this is not the case, the Competitor must make its Vehicle conform before passing technical scrutineering.

9) Vehicles must be presented to scrutineering ready to race and the safety equipment listed in Articles 47P1 and 47P2 must be presented to the Stewards in charge of controls.

No intervention will be allowed between the end of scrutineering and entry into the Parc Ferme.

All Vehicles which appear not to conform, or are not adapted to rally norms, during scrutineering may either be forced to change group or be refused a start (on decision of the College of Sporting Stewards).

In the last case the entry fees will remain the property of the Organiser.

42P3 ADMINISTRATIVE OBLIGATIONS

1) Each Crew member must present the following, valid, original documents during administrative checks:

- passport valid until 30/06/2013;
- visas: all participants must inform themselves of the necessary visas they will require for the countries they will cross, as a function of their nationality; when necessary, the participants will have to follow the mandatory steps to obtain the visas.
- authorisation to race abroad (issued by their national federation - ASN) for Competitors not having a licence delivered by the FFSA;
- permission of the owner to use the Vehicle, when they are not part of the Crew;
- valid national driving licences, corresponding to the category of Vehicle entered;
- in the truck group it is acceptable for only 2 Crew members to have a heavy goods Vehicle licence;
- a 2013 FIA international Competitor/Driver licence;
- vaccinations: no vaccination is compulsory. Recommended: diphtheria, tetanus, polio, meningitis (A & C), viral hepatitis A & B, typhoid and eventually rabies, etc.

2) Each Vehicle must be presented with the following original documents during administrative checks and scrutineering:

- valid log book or registration certificate (provisional registrations not acceptable);



- homologation file of Vehicles entered in T2 and T4.1;
- FIA technical passport supplied by the National Sporting Authority.

3) Each truck must also present the following original documents:

- driving licence for each Crew member valid for the Vehicle entered, (truck licence for at least 2 of the 3 Crew members);
- annual technical inspection report;
- acceptance report for conventional type bodies in case of separate approval.

4) Competitors undertake, on their honour, only to present valid documents, on pain of having a start refused.

No photocopies or theft or lose declarations of any kind will be accepted, on pain of having a start refused.

43P PROTESTS – APPEALS

1) All protests shall be lodged in accordance with the stipulations of the Code. They must be lodged in writing and handed to the Clerk of the Race together with the protest fee of €690, which shall not be returned if the protest is judged unfounded.

2) If the protest requires the dismantling and re-assembly of different parts of a Vehicle, the protest fee demanded will be decided as a function of the amount of dismantling required, as stated in the FFSA's general regulations.

3) The expenses incurred by the work and by the transport of the Vehicle shall be borne by the claimant if the protest is unfounded, or by the Competitor against whom the protest is lodged if the claim is founded.

4) If the claim is unfounded, and if the expenses incurred by the protest (examination, transport, etc.) are higher than the deposit amount, the difference shall be borne by the claimant. Conversely, if the expenses are less, the difference shall be returned to him.

5) The Competitors may lodge an appeal against decisions given, in conformity with the stipulations of the FFSA General Prescriptions and of Chapter XIII of the International Sporting Code.

The decision of the College of Sporting Stewards concerning problems of safety (ex: failure of a Competitor to respect the official itinerary, a damaged Vehicle, etc.) will take effective immediately, irrespective of any appeal.

The cost of a national appeal is: €3,300.

44P CLASSIFICATIONS

44P1 GENERAL

1) The Clerk of the Race bears the responsibility for timekeeping.

2) Penalties shall be expressed in hours, minutes and seconds. There will be a separate classification for T4 Vehicles. The final results shall be determined by adding together the times obtained in the Selective Sections and the penalties (incurred during the Road Sections and other penalties expressed in time).

The Crew which achieves the smallest total is proclaimed winner in the general classification, the next one being second, and so on. Classifications by group and class are drawn up in the same way.

3) In the case of a dead heat, the Crew which accomplished the best time for the last Selective Section will be proclaimed winner. If this is not enough to determine the winner, the times of the previous Selective Sections shall be taken into consideration, except for the Stages where no Selective Section has been run, in which case dead heats will be determined according to the order of starting numbers. This rule may be applied at any time during the Rally.

44P2 CLASSIFICATIONS

1) Definitions of the different classifications:

a) Partial classification of Selective Section: non official, distributed for information only (press, Teams, etc.).

b) Provisional classification of Selective Section: posted by the Clerk of the Race at 07.00pm the day after the stage in question. It becomes definitive after the period for complaints, which ends when the first racing Competitor has left the next Stage following this posting.

c) Provisional classification of Stage: posted by the Clerk of the Race at 07.00pm the day after the Stage in question. It becomes definitive after the period for complaints, which ends when the first racing Competitor has left the next Stage following this posting.

d) Provisional general classification of Event: posted by the Course Director at 07.00pm the day after the Stage in question. It becomes definitive after the period for complaints, which ends when the first Competitor on the course has left the next Stage following this posting.

e) The official classification of Stage and general classification of Event will be confirmed and signed by the College of Sporting Stewards, daily during the meeting of the College, after the period for complaints.

2) The various classifications will be posted each evening at the Bivouac at 07.00pm, in the catering area and at Race Control (PCO).

3) On the rest day the classifications of the previous Stage will be posted at 07.00pm and become definitive 30 minutes after.

4) Given that the classification becomes definitive as concerns the elements used to compile them, these elements and only these elements may be considered regarding any protest, once the final classifications are posted at the end of the Event.

5) The official classification of the Dakar 2013 will be posted on the official notice board at La Plaza de la Moneda, in Santiago, on 20th January 2013, at the entry of the Parc Ferme and at the Competitors department's tentParc Ferme.

They become definitive 30 minutes after their posting (reclamation period).

45P PENALTIES

1) Any failure to respect the texts of these Regulations for which the penalties are not mentioned will be recorded in a report to the Clerk of the Race, and the Stewards will decide what penalty to impose.

The penalties incurred are applied to the classification of the Leg and to the general classification of the Event.

Sporting penalties shall be applied to the time of Selective Sections and other time penalties will be applied to the general classification.

2) All fines will be donated to charity, with the agreement of the FFSA.

3) Where there is a difference between the text of an article and the table it is the text of the article that takes precedence.

SUMMARY OF PENALTIES	Art. n°	Request for ASN sanctions	Start refused	Disqualification	Time penalties	Financial penalties	Penalty Decisions at Stewards' discretion
Assistance in a closed space or private place	3P4.b						
- 1st offence					6h00		
- repeated				X			
Iritrack not ON during tests	3P4.b						X
Speeding in Bivouac and/or dangerous driving in Bivouac	3P4.c						Up to Disq.
Absence at 1st Briefing	3P5.b					€ 500	
3rd infraction, when speeding	3P18						Up to Disq.
Withdrawal of Crew member or admission of third person aboard Vehicle	12P1.3			X			
Air, water or land transport of a Crew member by a third person	12P1.4			X			
Safety equipment not FIA approved	12P1.5		X				
Not wearing compulsory safety equipment on a Selective Section	12P1.5			X			
Vehicle's characteristics not corresponding to those on entry form	14P1.5		X				X
Non administrative or technical conformity	14P3.1		X				
Absence or faulty positioning of a race number or plate	15P5					10% of entry fees (per infraction - Vehicle + Crew fee)	
Absence or faulty positioning of 2 or more race numbers or plates	15P5					20% of entry fee (Vehicle + Crew fee)	
Names of Crew members and national flag not features on each wing of the Vehicle	15P6					10% of entry fees (per infraction)	

SUMMARY OF PENALTIES	Art. n°	Request for ASN sanctions	Start refused	Disqualification	Time penalties	Financial penalties	Penalty Decisions at Stewards' discretion
Absence of bracelet	15P7					10% of entry fees (per infraction - Vehicle + Crew fee)	
Refusal of optional advertising	16P1.4						
- Per car						€ 17,600	
- Per truck						€ 21,280	
Absence ofr damage of advertising	16P1.5						
- 1st offence						10% of fees	
- Repeated						100% of fees (Vehicle + Crew fee)	
Absence of hole or seal	17P2.1					150 per hole	
Reconnaissance of route, tests or presence in the countries crossed	21P6						Up to refused start
- Possession of non- authorised notes in the Vehicle	22P2.3		X	or X			
- Possession of satellite photos inside the Vehicle			X	or X			
Non-respect of itinerary by a T4.3, after assistance on another Vehicle	23P2.1				50h00		
- T4.3 out of time for the start (missing Stage)	23P2.2						
- Repeated offence				X	100h00		
Presence of assistance or signposting in Public Zones	23P3.3						Up to Disq.
Missing WPM, WPS, WPE, WPV, DZ, FZ, CP	24P2.1				See Road Book		Up to Disq.
Exchange of GPS between Vehicles	24P3.2						Up to Disq.
Unauthorised linking of equipment	24P3.3						Up to Disq.

SUMMARY OF PENALTIES	Art. n°	Request for ASN sanctions	Start refused	Disqualification	Time penalties	Financial penalties	Penalty Decisions at Stewards' discretion
Use of GPS points other than those of the Organiser	24P3.7.a						X
Carry or possession of unauthorised systems of navigation	24P3.7.b						X
Presence of all unjustifiable wiring	24P3.7.c						X
Possession or use of forbidden system	24P3.7.d						Up to Disq.
Unblocking GPS using the code "WPM"	25P						
- 1st to 3rd utilisation for the 1st 20 cars and the 1st 10 trucks and the priority Drivers					6h00		
- 1st to 3rd utilisation for the others					3h00		
- 4th utilisation				X			
Straying more than 90 m from a DZ or a FZ	26P1 26P3				See Road Book		
- GPS not working due to Competitor	27P3						Up to Disq.
- GPS code not entered							
- Transport of a race Vehicle	28P1			X			
- Blocking a Vehicle to avoid overpassing							
Over speeding in Control Zones:	28P3.2						
- Between 1 and 15 kph					1' per impulsion	€ 100	
- Between 16 and 40 kph					2' per impulsion	€ 200	
Over 41 kph							
- 1st impulsion					5'	€ 300	
- 2nd impulsion					10'	€ 500	
- 3rd impulsion	28P3.3				15'	€ 700	
- Repeated offence						€ 1,000	
Non-payment of fines within 48 hours	28P3.4		X				

SUMMARY OF PENALTIES	Art. n°	Request for ASN sanctions	Start refused	Disqualification	Time penalties	Financial penalties	Penalty Decisions at Stewards' discretion
Speeding on Road Section with speed limit	28P4						
- Between 1 and 20 kph					30" per impulsion	€ 100	
- Between 21 and 40 kph					1' per impulsion	€ 200	
- Over 41 kph					5' per impulsion	€ 300	
- More than 3 successive impulsions					X Stewards' decision		Up to Disq.
Going over max speed, trucks	28P5.2						
- Between 141 and 145 kph					1' per impulsion	€ 100	
- Between 146 and 160 kph					5' per impulsion	€ 200	
Over 161 kph							
- 1st impulsion					20'	€ 300	
- 2nd impulsion					1h00	€ 1,000	
- 3rd impulsion				X			
Sentinel not working due to Crew	29P1.2						
- For the 1st 20 cars, 1st 10 trucks and A.S.O. Elite Drivers					1h00		
- For all the others						€ 300	
All Vehicle refusing to be overtaken	29P1.3						
- For the 1st 20 cars, 1st 10 trucks and A.S.O. Elite Drivers					15'		
- For all the others						€ 300	
Not respecting the Articles	29P2 29P3						X
Not making the area safe in the case of an accident	29P2.3						X
Crew involved in an accident with injuries	29P2.4						Up to Disq.
Disrespectful behaviour	29P4.1.a					€ 500	
Incorrect, fraudulent or unsporting action committed	29P4.1.b						
- Before the Event			X				
- During the Event							Up to Disq.

SUMMARY OF PENALTIES	Art. n°	Request for ASN sanctions	Start refused	Disqualification	Time penalties	Financial penalties	Penalty Decisions at Stewards' discretion
- Leaving wheel and/or punctured tyre - Repeated offence	29P4.1.c			X		€ 1,000 per wheel	
Transport of spare parts	29P4.2					€ 500 per spare parts	Up to Disq.
Vehicle transferred or sold in a country crossed	29P4.3						Banned on Dakar
Not respecting the rules for crossing borders	29P4.4						From € 500 up to Disq.
Not respecting the reexportation of a damaged Vehicle	29P4.5	X					
Not respecting a sensitive area - 1st infringement - repeated offence	29P4.7.a, c.			X	15'	€ 1,000	
- Broken fences' gate - Repeated offence	29P4.6.b			X	15'	€ 1,000	
Non-authorized use of satellite or GSM telephone	29P5.8						Up to Disq.
Use of any transmission or radio system not authorised	29P6						Up to Disq.
Competitor interferes with Iritrack	29P7.2						Up to Disq.
Incident due to Crew or attempt at fraud (boarded camera)	29P8.3						X
Offence committed by an assistance vehicle	31P1.4						Up to Disq.
Returning to Bivouac on unique track	31P2.d			X			
Forbidden assistance	31P3						Up to Disq.
Assistance by a non-accredited person or Vehicle - on a Road Section - on a Selective Section - Repeated offence	31P3.2			X	3h00 6h00		
- Assistance on a Selective Section - Repeated offence	31P3.3			X	6h00		Up to Disq.
- Presence of an assistance on DSS, ASS or race Road Section - Repeated offence	31P3.4			X	3h00		Up to Disq.
- Transport, delivery, etc. of spare parts - Repeated offence	31P3.5			X	6h00		Up to Disq.
- Assistance outside the Bivouac or in an enclosed place - Repeated offence	31P3.6			X	6h00		Up to Disq.
Signposting, transmission of information	31P3.7				3h00	€ 500	

SUMMARY OF PENALTIES	Art. n°	Request for ASN sanctions	Start refused	Disqualification	Time penalties	Financial penalties	Penalty Decisions at Stewards' discretion
Presence of a Vehicle on the itinerary before the race	31P3.8			X of all the assisted Competitors			
- Not authorised airborne assistance	31P3.9				6h00		Up to Disq.
- Repeated offence				X			
Presence of a Team Manager on the Selective Section	31P4.1						Up to Disq.
Transport of parts or tools by a Team Manager on the plane	31P4.1						Up to Disq.
Non-respect of refuelling safety rules	31P5.7						X
Refuelling by a T5, outside the Bivouac	31P5.9						X
Not respecting the rules for fuel	31P5.10						Up to Disq.
Not respecting the Article for a race Vehicle that has become an assistance Vehicle	31P6	X					
Change of engine block	32P			X			
Unauthorised modification of Time Card not approved by a Steward	34P2			X			
Absence of stamp or non-respect of chronological order	34P4						Up to Disq.
Loss or damage of Time Card	34P6				5'		
Entering a control area from the wrong direction and/or re-entering a Control Zone when the Time Card has already been checked	35P4						
- 1st infringement					10'		
- 2nd infringement					1 hour		
- 3rd infringement				X	or FP		
Failure to comply with the instructions of the Steward in charge of a control post	35P7						Up to Disq.
Time difference at a TC	36P1.8				1' per minute		
- Late at Start TC of a Stage or Selective Section, until closing of control	36P2				1' per minute		
- After closing of control	36P3			X			

SUMMARY OF PENALTIES	Art. n°	Request for ASN sanctions	Start refused	Disqualification	Time penalties	Financial penalties	Penalty Decisions at Stewards' discretion
Selective Section: Overrunning max time	36P4.2				Max time + FP		
- Crew not stopping in interview zone or incivility	36P7					€ 500	
- 2nd offence							X
- Not respecting the FIA norms	38P1			X			
- Infringement to Appendix L			X				
Driving in opposite direction on Selective Sections	38P1.2						Up to Disq.
Remaining on the starting line for more than 20" after the starting signal has been given	38P1.3				2'		
False start before controller's signal	38P1.5				1' mini		X
Stopping between yellow and stop panels	38P1.7				15'		
Not stopping at stop panels	38P1.9				1h00		
Crew refusing to start a Selective Section at its allotted time and position	38P1.12				15'		
Infringement of Parc Ferme Regulations	41P1.2				10h00 mini		Up to Disq.
- Work on Vehicle after start time	41P1.3				1' per minute		
- Over 30' late				X			
Crew presents after limits	42P1.1		X				
Vehicle not conform to safety regulations	42P1.3		X				
Responsibility / absence of identification marks	42P1.5			X			
Late at administrative checks, per minute:	42P2.2						
- 1st hour						€ 50	
- Subsequent hours							€ 80

SUMMARY OF PENALTIES	Art. n°	Request for ASN sanctions	Start refused	Disqualification	Time penalties	Financial penalties	Penalty Decisions at Stewards' discretion		
Late at scrutineering:	42P2.3					€ 15			
- up to 30'									
- 30' to 1 hour									
- following hours						€ 45			
Absence at Irirack or Sentinel training	42P2.5					€ 500			
Instrument or brackets not pre-installed (in Le Havre)	42P2.6						Boarding refused		
Instrument or brackets not pre-installed (in Lima)	42P2.7					€ 150 per instrument			
Vehicle does not conform at scrutineering	42P2.9		X				X		
Presentation of copies of documents or non-valid documents	42P3.4		X						
Amount of protest	43P1					€ 690			
Amount of appeal - national	43P5					€ 3,300			
Lack of survival equipment at start	47P1.2				1' per minute				
- to comply									
- Over 30'								X	
- 1st repeated offence									X
- 2nd repeated offence									X
- Not wearing FIA approved safety equipment on Selective Sections	47P2.1								
- Failure to present this equipment at scrutineering								X	
Failure to inform or withdrawal	47P3.2	X				X	X A.S.O. refuses entry of Crew		
Letting off distress beacon	47P3.2								
- without medical reason								X	
- if the Organiser intervenes								X	X
Transport of a Competitor by any means of transport	47P3.5			X					

1) General Classification

1st prize	1 trophy	6th prize	1 trophy
2nd prize	1 trophy	7th prize	1 trophy
3rd prize	1 trophy	8th prize	1 trophy
4th prize	1 trophy	9th prize	1 trophy
5th prize	1 trophy	10th prize	1 trophy

2) Groups' Classifications

4x4 T1 GROUP		2 WD / T1 GROUP		SCORE	
1st prize	1 trophy	1st prize	€ 5,000 + 1 trophy	1st prize	€ 5,000 + 1 trophy
2nd prize	1 trophy	2nd prize	€ 3,000 + 1 trophy	2nd prize	€ 3,000 + 1 trophy
3rd prize	1 trophy	3rd prize	€ 2,000 + 1 trophy	3rd prize	€ 2,000 + 1 trophy

T2 GROUP		T3 GROUP		T4 GROUP	
1st prize	€ 5,000	1st prize	€ 3,000	1st prize	1 trophy
2nd prize	€ 3,000	2nd prize	€ 2,000	2nd prize	1 trophy
3rd prize	€ 2,000	3rd prize	€ 1,500	3rd prize	1 trophy

3) Various Classifications

100% FEMININE CLASSIFICATION		NRJ CHALLENGE	
1st prize	€ 3,000 + 1 trophy	1st prize	€ 5,000 + 1 trophy

100% FIRST PARTICIPATION		SOLO CLASSIFICATION	
1st prize	€ 5,000 + 1 trophy	1st prize	1 trophy
2nd prize	€ 2,000 + 1 trophy	2nd prize	1 trophy
3rd prize	€ 1,000 + 1 trophy	3rd prize	1 trophy
T4.A GROUP		T4.3 GROUP	
1st prize	€ 3,000 + 1 trophy	1st prize	€ 3,000 + 1 trophy
6x6 CHALLENGE		LESS THAN 10 LITRES CHALLENGE	
1st prize	1 trophy	1st prize	1 trophy

4) Trophies will be awarded to winners at the official prize giving to be held on 20th January 2013, at the podium in the Plaza de la Moneda in Santiago, from 10.00am. Prize money will be sent to Competitors by 31st March 2013 at the latest.

5) The prizes are cumulative. The total amount of prizes given out is 58,500 €.

47P1 SURVIVAL EQUIPMENT

1) For safety reasons Crews must carry with them in their Vehicles the following material:

- 1 lighter
- 1 strobe lamp
- 1 torch
- 3 hand-held flares : 1 night hand flare, 1 night / day hand flare, 1 light stick
- 1 compass
- 1 distress mirror
- 1 reserve of 5 litres of water per person + 1 1.5 litre 'camelback' type drinking system per person at the start of each stage
- 1 seat belt cutter for each person, accessible from the seat with harness fastened
- 1 foil survival blanket per person
- 1 very loud horn, that functions perfectly throughout the event
- 1 safety triangle + 2 yellow fluorescent jackets
- 1 Sarsat distress beacon
- 1 Iritrack
- 1 GPS - Sentinel

2) All Crews coming to the start of a Stage without all the compulsory survival equipment (water and safety material) will have a time limit of 30 minutes in which to conform to these Regulations. The time taken will be considered as time late for a Time Control and will result in penalties of 1 minute for every minute late. A new start time will be given. All lateness over 30 minutes will result in Disqualification.

Repeat offenders on following Stages will receive penalties up to and including Disqualification, to be decided upon by the College of Sporting Stewards. Repeat offenders will be readmitted once only.

3) Safety and survival equipment must be accessible without dismantling, so that the Race Direction may carry out checks before the start of each Stage.

The location of the distress beacon must be shown on the outside of the Vehicle using a sticker supplied by the Organiser.

The instructions for the various items of safety equipment must be respected and Competitors will be tested on their knowledge, during the administrative checks.

4) If the windscreen is broken and it cannot be repaired, the Crew will be allowed to take the start of following Stages without a windscreen, with a jet helmet and bike goggles (safety net not compulsory).

47P2 EQUIPMENT

1) The wearing of a FIA approved fireproof overall suit, undergarments, balaclava, socks, shoes, gloves (Driver) (complying to FIA norms 8856-2000, Appendix L chapter 3 FIA) is compulsory during Selective Sections, on pain of Disqualification and strongly recommended for off-road and tracks' Road Sections.

The wearing of a harness is compulsory throughout the Event.

This equipment must be presented during scrutineering, on pain of being refused a start.

2) The wearing a "Hans" type head restraint system is mandatory and must comply with Appendix L, chapter 3, FIA or with Score Regulations.

47P3 WITHDRAWALS – DISQUALIFICATION

1) In the case of withdrawal, it is imperative that the Crew informs, by all possible means, and as quickly as possible, the PCO on +33 1 41 33 15 81.

Failure to respect this important safety clause will result in the refusal of all future applications to take part in any Cross-Country Rally organised by A.S.O.



In addition, failure to respect the obligation to inform the Organisers in the case of withdrawal, where search operations are carried out, will result in the Crew in question being liable for the financial costs of search operations and the request to the Crews' National Sporting Authority for further sanctions, upon the discretion of the College of Sporting Stewards.

2) In the case of withdrawal due to mechanical breakdown, the Crew must imperatively spend a night out on the track before letting off its Sarsat distress beacon, if they have not yet been picked up by the Sweeper Truck.

The letting off, without medical reason, of the distress beacon, will result in Disqualification, as well as the financial liability of the Competitor concerned, in the case of intervention by the Organisation.

3) In the case of withdrawal it is the Crews' responsibility to ensure that safety material is returned as fast as possible to the relevant suppliers (Sarsat distress beacon, Iritrack, Sentinel, GPS and associated accessories). This equipment must be returned to the representatives of the suppliers of this equipment, located at the Safety Center. Under no circumstance can A.S.O. be held responsible for the disappearance or loss of this equipment, too often handed to a third party.

4) In the case of withdrawal, it is the responsibility of the Crew or Competitor to organise the transport of their Vehicle to Port of Santiago (Chile). The cost of removing the Vehicle for the Selective Section, as well as the cost of transport, is entirely at the charge of the Crew or Competitor.

The Organisers can provide Competitors with the contact details of companies able to retrieve Vehicles from the track and/or transport them to Santiago.

5) The transporting of a Crew member by helicopter or aboard any other means of transport during all or part of a Stage will result in Disqualification.

Crews that have withdrawn may under no circumstance be transported by the Organisation during all or part of the remaining route.

47P4 CLOSING THE TRACK

1) The Organisers' 'Sweeper' Vehicles will close the Rally track.

As it is impossible to guarantee that the Sweeper Truck will pass by the exact spot where a Vehicle has broken down, it is the Competitors' responsibility to indicate their position to the PCO using the safety equipment aboard. The Sweeper Trucks will pick up Competitors whose Vehicle has broken down, but will under no circumstances tow the broken down Vehicle.

2) Competitors who refuse to take a place aboard the Sweeper Truck do so entirely at their own responsibility and must sign a disclaimer which will be given to them by the members of the Organisation in charge of closing the track.

No action may be taken against the Organisers in the case where Competitors refuse to board the Sweeper Truck. However, refusal to sign the disclaimer will not result in Disqualification.

47P5 DEPOSITS- LETTER OF UNDERTAKING

There will be no requirement to pay a deposit to the Organiser.

For safety equipment (GPS, Iritrack, Sarsat distress beacon, Tripy), a deposit will be paid directly to the supplier.

Competitors will be required to sign a letter at administrative checks by which they undertake to:

- respect the Regulations and the safety rules;
- pay for any damage caused to safety and navigation equipment supplied to them for the Rally (GPS, Iritrack, Sarsat distress beacon, Tripy);
- return safety and navigation equipment supplied to them for the Rally (GPS, Iritrack, Sarsat distress beacon, Tripy) by the latest at the end of the Rally;
- return all material lent to them for the Rally;
- respect the withdrawal rules.

TECHNICAL REGULATIONS

PREAMBLE

The Technical Regulations for the 35th Dakar Perú - Argentina - Chile conforms with the 2012 FIA Regulations. There are however some exceptions, which you will find detailed in the following pages.

The FIA technical Regulations can be consulted on their website: www.fia.com
(Click on: FIA Sport / Regulations / FIA Cross-Country Rallies. Then articles 281, 282, 283, 284, 285, 286, 287.)

FIA:

Tel: 00.41.22.544.44.00 (Switzerland)

Car Technical Responsible:

Will be communicated via a Bulletin.

CARS

ARTICLE 1P – SPECIFIC REGULATIONS FOR MODIFIED CROSS-COUNTRY CARS (T1 GROUP)

1P1 WIDTH

For 4x2 cars, the maximum width of bodywork is 2 400 mm without rear view mirrors.

1P2 MINIMUM WEIGHT

Cars are subject to the scale of minimum weights in relation to cylinder capacity and in accordance with Article 282-3.2 except for single turbo diesel Vehicles whose nominal cylinder will be affected with a 1.3 coefficient.

1P3 PETROL ENGINE

Engines must be derived from a car model able to be homologated in Group N, A (i.e. produced in a quantity of more than 2 500 units in 12 consecutive months), GT (2012 GT homologation Regulations) or T2.

The following parts must be kept original:

- Engine block (except machining and modification to fit i.e. gear box, engine mounts, generator, starter);
- Crankshaft;
- Connecting rods;
- Pistons;
- Valve train (belt or chain, pulley);
- Cylinder head(s), completely assembled unit(s); it is allowed to do certain machining and modification for the set-up of mounting supports.

A workshop manual from the relevant manufacturer will be used as reference for conformity check. It is allowed to mechanically block standard engine variable functions (thus to delete this function), for example a valve train, camshafts, an inlet.

The **engine block** must be kept original, but can derive from another model from the same manufacturer.

Flywheel is free.

Cylinder head cover(s) is(are) free provided it has a weight at least equal to that of the series cylinder head cover.

Intake manifold is free.

Exhaust manifold is free.

Starter is free provided it is issued from a series production.
Its installation inside the engine compartment is free.

Generator may be replaced by another one.

The diameter of the throttle valve cannot be bigger than the standard engine's one.

Ignition:

Spark plugs are free as are spark plugs / coil high tension cables.
The electronic control unit and the electronic ignition components are free.

Engine loom is free.

The fitting of **fuel coolers** is authorised on the return circuit to the tank.

1P4 PETROL ENGINE, AIR RESTRICTORS

All the air necessary for feeding the engine must pass through a restrictor, which must comply with Article 284-6.1, except for its internal diameter.

It is possible to use 2 air restrictors provided that the diameter normally used for one restrictor is divided by 1.4142.

No "by-pass", by any mean, is allowed.

1P4.1 Restrictor for petrol engines

All petrol engines must be fitted with an air restrictor with a maximum internal diameter (in mm) of:

PETROL	DAKAR 2013
Prototype*	34
Standard (2 or 4 valves)	36
>5400cc 4x4 FIA / Score	36
4x2 FIA	39
>5400 4x2 Score	37,2

* Only for drivers not included within A.S.O.'s "Elite" list. The latter cannot drive a Vehicle equipped with a prototype engine.

1P5 FUEL COOLING

The fitting of fuel coolers is authorised on the return circuit to the tank.

1P6 SUPERCHARGED DIESEL ENGINE

Engines must be derived from a car model able to be homologated in Group N, A (i.e. produced in a quantity of more than 2500 units in 12 consecutive months), GT (2012 GT homologation



Regulations) or T2.

The following parts must be kept original:

- Engine block (except machining and modification to fit i.e. gear box, engine mounts, generator, starter);
- Crankshaft;
- Connecting rods;
- Pistons;
- Valve train (belt or chain, pulley);
- Cylinder head(s), completely assembled unit(s); it is allowed to do certain machining and modification for the set-up of mounting supports.

The workshop manual from the relevant manufacturer will be used as reference for conformity checks.

The **engine block** must be kept original, but can be derived from another model from the same manufacturer.

Flywheel is free.

Cylinder head cover(s) is(are) free provided it has a weight at least equal to that of the series cylinder head cover.

Intake manifold is free.

Exhaust manifold is free.

Starter is free provided it is issued from a series production.
Its installation inside the engine compartment is free.

Generator may be replaced by another one.

Supercharging air exchanger

A new exchanger may be homologated respecting the following conditions:

- It must come from a car model able to be homologated in Group A or T2 (i.e. produced in a quantity of more than 2500 units in 12 consecutive months);
- The air/air intercooler must have a maximum interchanger core volume V_{1max} of 12 dm³;
- The total volume of the core is determined by its external dimensions (Length x Width x Thickness);
- It is permitted to modify the air inlets and outlets cases of the new exchanger, for the sole purpose of facilitating the implantation in the car. The turbo/exchanger and exchanger/engine air ducts are free; their maximum internal diameter is 80 mm;
- Exchangers of the air/water type are prohibited, unless the system is derived from a car model that can be homologated in Group A (i.e. produced in a quantity of more than 2500 units in 12 consecutive months) in this case, no modifications are authorised except for connections' cases.

For 2013, it is authorised to combine an air/water type exchanger system with an air/air type exchanger system.

Air ducts are free but the maximum internal volume between the restrictor and the intake manifold set-up front is set at 15 dm³.

One fan may be added on the exchanger.

Turbocharger

A **Single Stage Turbocharger** system with 1 (straight 4, 5 or 6 cylinder layout) or 2 (V6 or V8 cylinder layout) single stage turbos and their control system (waste gate type or other types) can be replaced by any other single stage turbo charger which derives from a car model that can be

homologated in Group N, A (i.e. produced in a quantity of more than 2500 units in 12 consecutive months), GT (2012 GT homologation Regulations) or T2. The actuators and control system levers are free.

In case a **Two Stage Turbocharger** system with 1 (straight 4, 5 or 6 cylinder layout) or 2 (V6 or V8 cylinder layout) turbocharger systems will be used, each stage will have to be issued from the Engine's brand but could come from different models. The actuators and control system levers are free.

Local machining of the turbocharger casing is permitted for the fitting of the air ducts.

Particle filters are prohibited.

Chain cover

The chain cover is free.

1P7 DIESEL ENGINE, AIR RESTRICTOR

1P7.1 Air restrictor

All the air necessary for feeding the engine must pass through a restrictor, which must comply with Article 284-6.1, except for its internal diameter.

It is possible to use 2 air restrictors provided that the diameter normally used for one restrictor is divided by 1.4142.

No "by-pass", by any mean, is allowed.

1P7.2 Restrictor for supercharged diesel engines

All supercharged diesel engines must be fitted with an air restrictor with a maximum internal diameter of:

<i>DIESEL</i>	DAKAR 2013
Prototype*	36
Standard single turbo	39
Standard twin turbo	38

* Only for drivers not included within A.S.O.'s "Elite" list. The latter cannot drive a vehicle equipped with a prototype engine.

1P8 FUEL COOLING

The fitting of fuel coolers is authorised on the return circuit to the tank.

1P9 POSITION (4x4)

The crankshaft must be ahead of the middle of the wheelbase and installed longitudinally for tubular frame chassis.

The minimum height between the crankshaft axis and the reference point situated on the front axle centre line is 190 mm.

All modifications designed to achieve this value are authorised for a monocoque body chassis.

1P10 WHEELS AND TYRES

The use of any system for inflating / deflating the tyres is forbidden, except for 4x2.

In order to adjust the tyre pressure, any air going in or out must pass through a conventional type of valve coming from a series light utility vehicle and having a VG5 type screw thread.

Screened and/ or recorded tire pressure control is forbidden.

1) For pilots within the A.S.O. Elite list concerned by the technical Regulations with 4-WD vehicles, a single tyre pattern is authorised for each Vehicle, during the whole Race.

a) Only these tyres may be used during the Rally.

b) A pattern is associated with each dimension and, if left tyres are different from right tyres, their patterns must be symmetrical.

2) 2-wheel drive T1 vehicles must use tyres from a tyre manufacturer's commercial catalogue.

3) Re-grooving of new and used tyres is forbidden.

4) Complete wheels must have a maximum diameter of 940 mm for 2-wheel drive cars and 810 mm for 4-wheel drive cars.

ARTICLE 2P – 2013 CROSS-COUNTRY RALLIES GENERAL PRESCRIPTIONS

2P1 TEMPORARY « ELITE » DRIVERS LIST

2P1.1 List of "Elite" Pilots concerned by A.S.O.'s Technical Regulations:

Surname	Name
AL- MUTAIWEI	HHALIFA
AL-ATTIYAH	NASSER
ALVAREZ	LUCIO
CHABOT	RONAN
CHICHERIT	GUERLAIN
COX	ANDREW
DE VILLIERS	GINIEL
DELAHAYE	REGIS
DEPPING	DIETER
ERRANDONEA	BERNARD
GARAFULIC	BORIS
GARLAND	BRUCE
GORDON	ROBBY
HENRARD	STEPHANE
HOLOWCZYC	KRZYSZTOF
KAHLE	MATTHIAS
KUZNETSOV	ILJA
LAVIEILLE	CHRISTIAN
LEAL DOS SANTOS	RICARDO
MAGNALDI	THIERRY
MILLER	MARK
MIRONENKO	ALEXANDER
NEVES	MAURICIO JOSE
NOVITSKIY	LEONID
PATISSIER	ISABELLE
PETERHANSEL	STÉPHANE
ROMA	NANI
SAINZ	CARLOS
SALAZAR	ELISEO
SCHOTT	STEPHAN
SOUSA	CARLOS

Surname	Name
SPINELLI	GUILHERME
TEN BRINKE	BERNHARD
TERRANOVA	ORLANDO
THOMASSE	PASCAL
VAN DEIJNE	TONNIE
VAN LOON	ERIK
VASILYEV	VLADIMIR
VIGOUROUX	ERIC
VOS	DUNCAN
WEVERS	ERIK
SZALAY	BALAZS
ZHOU	YONG

2P1.2 Only drivers using T1 cars will be considered to be "Elite".

2P1.3 The Organiser reserves the right to revise this list at any moment and at its own discretion.

2P2 TYRES

For A.S.O. "Elite" Drivers with 4-WD vehicles, a single tyre pattern is authorised for each "Elite" Driver.

2P3 ADVERTISING

The engine manufacturer is not necessarily linked to the cars- and/or the manufacturer's name.

ARTICLE 3P – INTEGRATION OF VEHICLES CONFORMING WITH 2006-2010 US SCORE REGULATIONS

Definition of the categories and classes: article 9P2 of the supplementary Regulations.

3P1 ADMISSIBLE VEHICLES

Technical details, Group OPEN vehicles:

All 2 or 4 wheel drive vehicles conforming to Score Regulations (2006-2010 Score International Off-road Racing Rules and Regulations) are admissible, with the exception of classes ½-1600 and 5-1600, under the following conditions: Respect of articles Appendix J – FIA 285.7 and 285.3.

3P2 CONCEPTION OF THE ROLL CAGE

The roll cage must conform to Score Regulations (see article CR33) or Article 283/8 Appendix J – FIA.

3P3 ROLL CAGE DIMENSIONS

The dimensions of the tubes must conform with Score Regulations (see article CR33) and more precisely respect the following dimensions of the following table:

Vehicle weight	Open cockpit	Closed cockpit
1,350 kg to 1,360 kg	∅ 44,4 mm x 3,05 mm	∅ 38,1 mm x 3,05 mm
1,360 kg to 1,810 kg	∅ 50,8 mm x 3,05 mm	∅ 44,4 mm x 3,05 mm
Over 1,810 kg	∅ 57,2 mm x 3,05 mm	∅ 50,8 mm x 3,05 mm



3P4 WEIGHT

Group OP.1: For 2-wheel drive, the minimum weight is 1.400 Kg

Group OP.2: The minimum weight is 2.800 Kg

Or respect of art. 285.3, appendix J FIA

3P5 TYRE SIZE

Tyres must respect the following sizes:

- **950 mm max, for 2-wheel drive** Group OP.1 vehicles and for Group OP.2 vehicles.
- **840 mm max, for 4-wheel drive** Group OP.1 vehicles, on 15 inch rims.
- Or respect of article 285.8, appendix J FIA and Art. 10P of the specific Regulations.

3P6 VEHICLE WIDTH

Group OP.1: For 4-wheel drive, the max width is **2,20 metres**.

For 2-wheel drive, the max width is **2,40 metres**.

Group OP.2: The max width is **2,20 metres**.

3P7 RESTRICTOR

Group OP.1 and OP.2 vehicles must conform to Article 1P1 of the current technical Regulations (2 wheel drive only), as well as with Appendix J FIA, Article 285.4.1.1.1 and Article 285.4.1.1.2.

3P8 INFLATION/DEFLATION

Automatic inflation/deflation will be allowed for Group OP.2 vehicles if it exists as standard and providing the system has not been modified in any way.

Automatic inflation/deflation systems are forbidden for vehicles in Group OP.1 4x4 even if fitted as standard (appendix J FIA, article 285.8).

3P9 MISCELLANEOUS SAFETY EQUIPMENT

Vehicles must conform to appendix J FIA, Article 283, articles 7, 15, 16, 17, 18 and 19.

3P10 FUEL TANKS

Fuel tanks must conform either to Article CR21 of the Score Regulations or with Appendix J FIA, Article 293, article 14.

3P11 FUEL

Fuel used must conform with Appendix FIA, Article 282, article 9 and the specific Regulations of the event, article 31P6.

ARTICLE 4P – T2 GROUP

The Technical Regulations of the 35th Dakar Perú – Argentina – Chile conforms to the 2012 FIA Regulations. There are however some exceptions, which you will find detailed in the following pages.

ARTICLE 5P – T3 GROUP

All T3 category Vehicles will have to be equipped with a flagpole of a 2.5 metres high minimum on the extremity of which will be set a flag. The flagpole will be delivered by the Organiser during administrative checks and scrutineering in Lima.

ARTICLE 6P – TRUCK REGULATIONS (T4.2 GROUP)

All the T4.2 engaged in the Dakar will have to have an A.S.O. technical passport valid for 5 years after the visit of the A.S.O. authorized truck category responsible.

Category responsible:

Fabien CALVET. Tel: + 33.6.98.92.20.02 (France) – E-mail: fabien.calvet@mac.com

6P1 GENERAL

6P1.1 Homologation

Any T4.2 truck corresponding to the current Regulations' criteria and its appendices and which elements are freely available in retail shops, is eligible.

A matriculation certificate for a road use is compulsory; the annual check must be valid on pain of being refused to start.

6P1.2 Truck elements' characteristics

The association of different parts is allowed and all parts (cab, engine, gearbox, transfer box, rigid axles, etc.) must be from a minimum of 200 copies produced batch and come from standard elements freely available in retail shops.

Vehicle's brand is defined by the truck's cab.

These Technical Regulations govern competition for 2 to 4 cargo axles.

Vehicles must comply with A.S.O. technical passport for T4-2 Group.

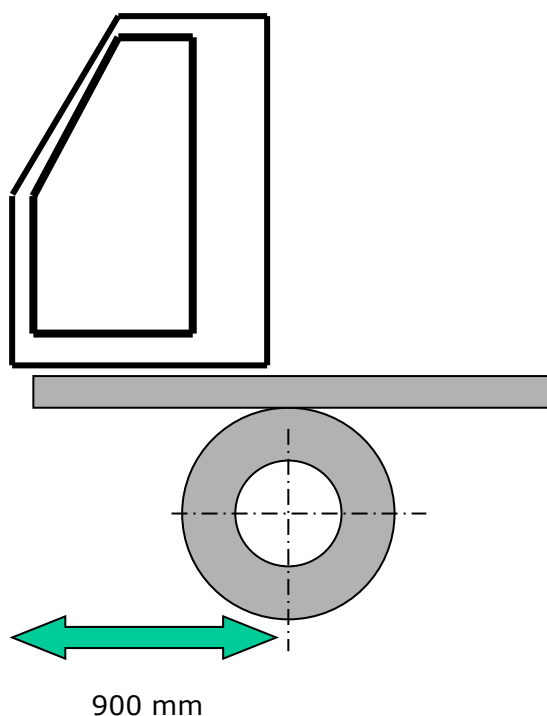
6P2 CAB'S CHARACTERISTICS

6P2.1 General

The cab must be standard (dimensions and material from production). The cab must correspond to the vehicle standard gross weight when empty (for instance, it is not allowed to fix a Midlum cabin on a Kerax truck).

This cab's doors must be exactly as the production ones' dimensions. It is strictly forbidden to cut wheels' cross or to modify the inferior part of the truck's doors.

The cab's front part (or bonnet's front part for a bonnet truck) must be set to a minimum distance of 900 mm from the centre of the front wheel axle.

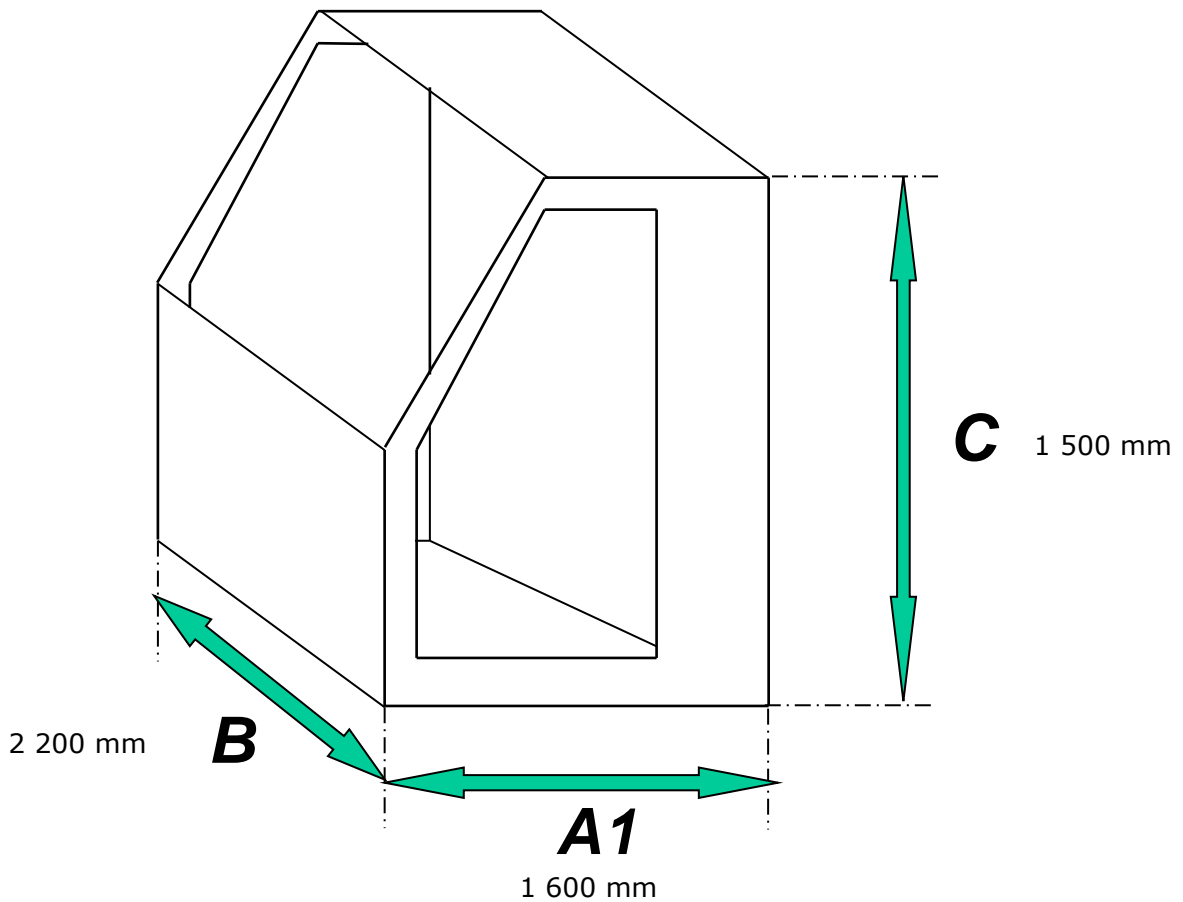


It is forbidden to push the cab back further than 900 mm. However, if the distance of the cab is inferior to 900 mm on the standard model, the truck will be admitted. The manufacturer must prove a commercialisation of at least 200 vehicles of this type for an individual and commercial use.

6P2.2 Cab's dimensions

Cab's minimum dimensions must respect the following specs:

- A1 mini = 1 600 mm
- B mini = 2 200 mm (Distance measured from one side of the door's hinge to the other)
- C mini= 1 500 mm



6P2.3 Safety elements

It is necessary to strengthen the passengers' seats' fixation; in order to do this, it is allowed to modify the engine tunnel.

All vehicles must have a protective bulkhead made of non-flammable material located between the engine/transmission and the driver's compartment capable of preventing the passage of fluids or flames in the event of fire. All the gaps must be sealed. The only oil lines which may run within the cab are those leading solely to temperature and pressure gauges as well as to the gearbox assistance. Such lines must be either metallic or aviation type lines.

All window operating mechanisms must function as originally designed by the manufacturers (e.g. manufacturers' wind-down windows must remain as wind-down windows). A protective cap (not opaque) must be installed on lateral windows to prevent from glass projection in case of an accident.

All vehicles must be fitted with a windscreen wiper and a washer. These must be maintained in a working condition at all times.

Steering column can be modified to make driving position better adapted to race mode. This modification must be validated by the technical delegate. This system must be validated during vehicle's technical scrutineering and comply with a road use.

Any steering lock system fitted to the vehicle must be removed.

The location of the parking brake control must be clearly indicated by a notice inside the cab at least 20 cm in width. The parking brake control must be operable by the driver while normally seated with seat belts fastened.

6P2.4 Cab's additional parts

It is allowed to modify production materials of the cab's additional parts while strictly keeping the original shape (front grid, air deflector, sun visor, etc.) but truck must keep standard model aspect.

6P3 SAFETY STRUCTURE

6P3.1 Cab

6P3.1.1 General

An internal roll cage must be fitted. The basic purpose of such a roll cage is to protect the driver and passenger if the vehicle is involved in a serious accident.

Minimum acceptable roll cage requirements are detailed in these Regulations but the following observations should be noted:

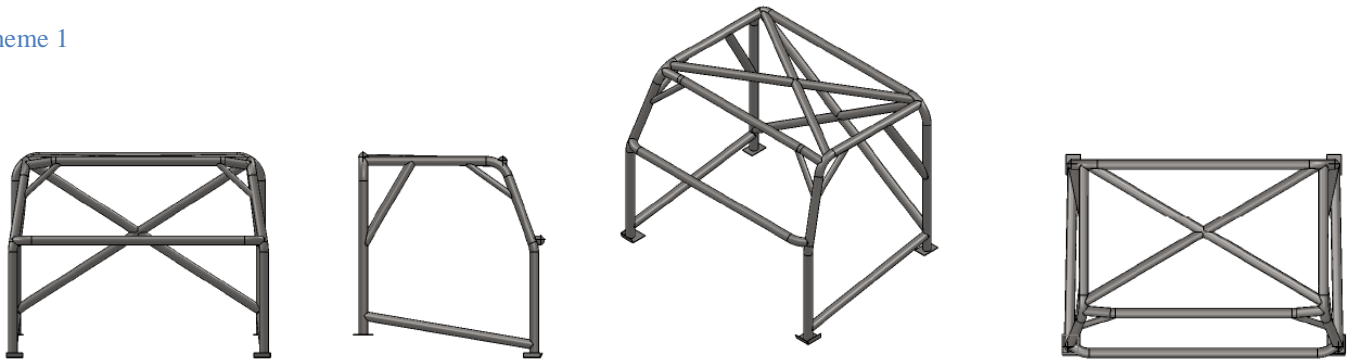
- a) The essential characteristics of a safety cage come from a finely detailed construction, suitable fixation to the cab and snug fitting against the bodywork;
- b) It is recommended that mounting feet be made as large as possible in order to spread loads over the maximum area. It is also advisable, where possible, to weld the cage to the cab structure (e.g. to the screen and door pillars) as far as possible. This greatly increases strength and rigidity;
- c) All welds should be of the highest quality possible, with full penetration (preferably arc welding and in particular under protecting gas) ;
- d) A longitudinal member (door reinforcement) must be fitted at each side of the vehicle. These members may be removable. This lateral protection must be set as high as possible but not higher than one third of the total height of the door measured from its base.

These requirements are a minimum. It is permitted to fit extra elements or reinforcements in addition to the basic requirements.

A cross or a V on the roof roll bar is mandatory.

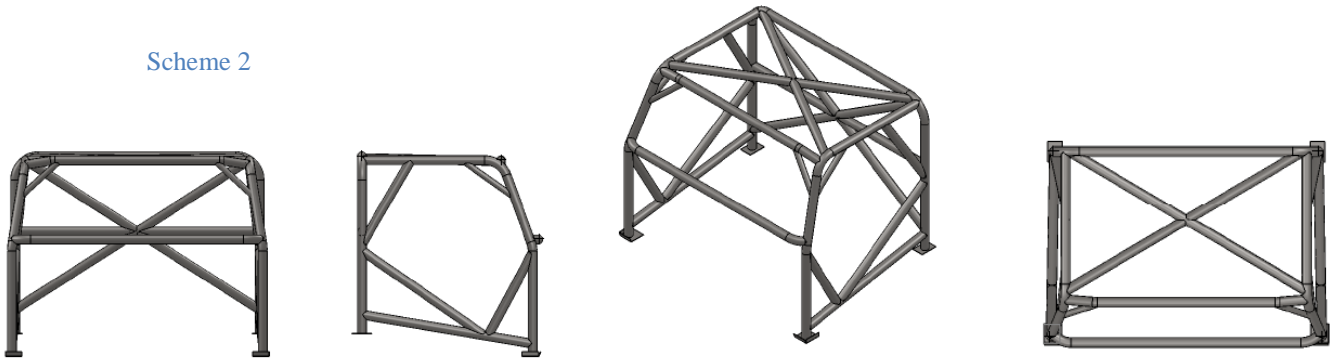
- The following scheme shows the mandatory minimum prescriptions with regards to the cab's safety structure.

Scheme 1

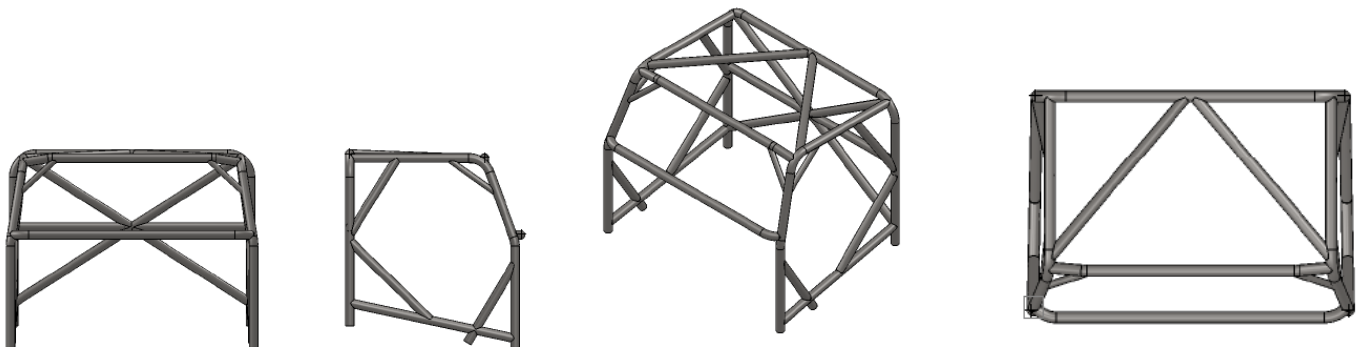


- Schemes 2, 3 and 4 represent different options allowed for the cab's safety structure

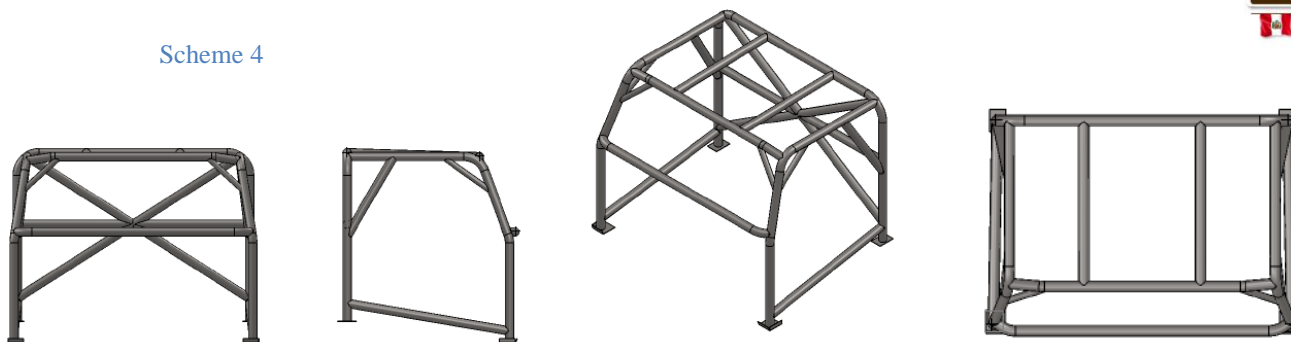
Scheme 2



Scheme 3



Scheme 4



6P3.1.2 Technical Specifications

Each roll bar must be in one piece and must be free from unevenness and cracks. All the parts of the cage must be welded together.

The rear diagonal must have its top fixing on the driver's side of the cab.

It is allowed, and even recommended, to fit additional struts to the cage.

Reinforcing plates with an area of at least 200 cm² and a minimum thickness of 3 mm must be fitted so that the cab floor is jammed between the roll bar mounting feet and the reinforcing plates.

The reinforcing plates must be screwed by a minimum of 3 screws of 8.8 ("S" Grade) specification and with a diameter of 12 mm screws. It is permitted to increase the number of screws and to weld the cage to the cab shell (e.g. to windscreen pillars).

All bars must be cold drawn seamless steel tube with a minimum tensile strength of 340 N/mm.

6P3.1.3 Dimensions

Minimum permitted tube sizes are as follows:

- 57 mm outside diameter x 4.9 mm wall thickness
- 60 mm outside diameter x 3.2 mm wall thickness
- 70 mm outside diameter x 2.4 mm wall thickness

Every tube must have a hole of 5 mm diameter, drilled in an easily visible position allowing the technical delegate's check.

6P3.1.4 Cab lock down system

To prevent cab tilt, original tilt lock mechanism must be kept but it is also mandatory to add an additional security system. This system must be fixed between cab rail (or roll bar) and truck's chassis. Counterplates must be installed to strengthen the system. These plates must be fitted with 4 bolts of 12 mm minimum.

The weakest part of the device will be either one screw or steel bold pin of at least 16 mm diameter or two steel or bold pins of at least 12 mm diameter. Steel cables are allowed on each side of the cab, with a minimum diameter of 12 mm (or equivalent section). They will be of a sufficient length to allow clearance between the cab and the chassis.

Vehicles with bonnets must be fitted with an additional locking device, in addition to the normal bonnet lock, to prevent the bonnet from opening in case of failure of the normal lock (attachment by means of « American » steel pins). Such devices must be properly locked while the vehicle is in movement.

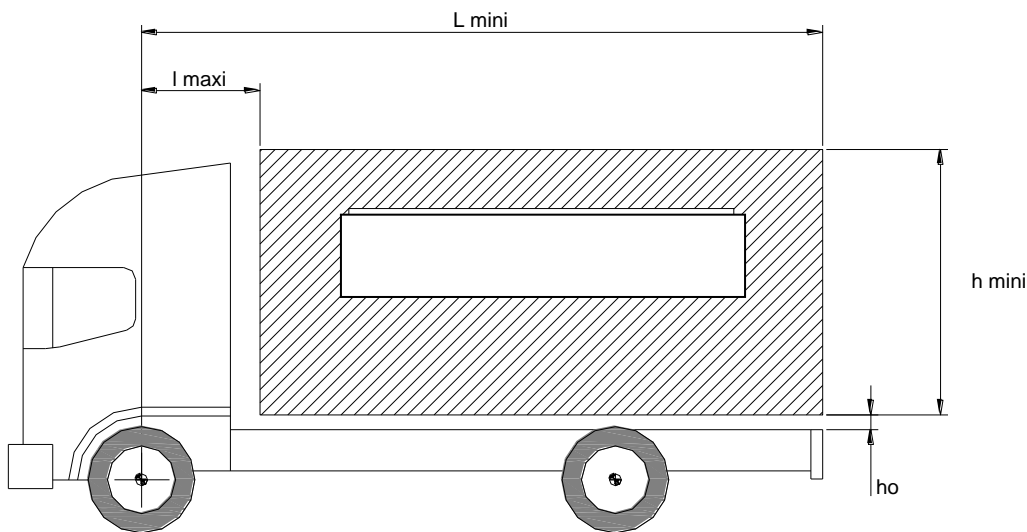
6P4 CARGO

6P4.1 General

The cargo must be validated by A.S.O. truck category responsible during the A.S.O. passport validation.

A cabover truck with a more than 10 liters engine must respect the following characteristics: $L = 4000$ mm, $I = 1300$ mm, $h = 1650$ mm, $h_o = 100$ mm.

Bonnet trucks or trucks with a less than 10 liters engine must respect the following dimensions: $L = 3000$ mm, $I = 1300$ mm, $h = 1650$ mm, $h_o = 100$ mm.

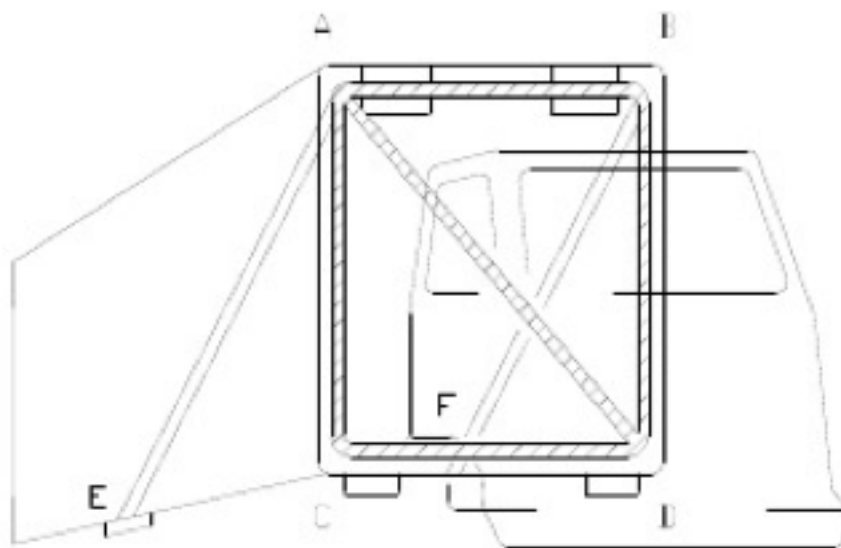


The transporting of « merchandise » can be done under the entire responsibility of the competitor. However, the Technical Stewards may check the quality of the load to make sure the safety standards are honored.

The transporting of certain quantities of dangerous materials is governed by official Regulations (road use).

6P4.2 Rollbar Specifications

A rollbar will be placed in front of the cargo. This rollbar will be fixed as shown below. This item must be as close as possible from the cab and cannot be further than 1500 mm.



The rollbar will have to be completely closed ABCD. It will have to follow in-depth the contour of the inside bodywork and its minimum height must be at least equal to that of the highest part of the cab or of its external rollbar. The rollbar ABCD must be made rigid by at least one diagonal AD or BC, but the fitting of two diagonals AD and BC is authorised.

This rollbar will be fixed on one hand by steel plates welded to the tube and bolted counterplates at floor level, as near as possible to corners C and D, and on the other hand, in the same way, to the vertical wall of the bodywork (except in tarpaulin type trucks) nearby corners A and B.

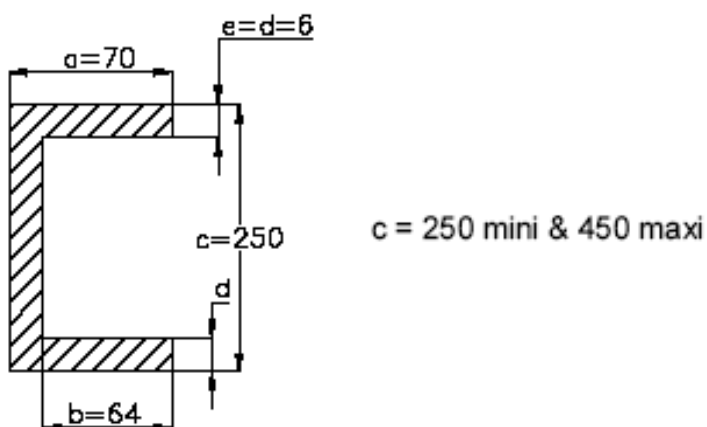
If the floor is not strong enough, this anchoring must be carried out on the chassis.

The rollbar must be held up by two rectilinear tension rods fixed at A and B and bolted to the floor of the vehicle with plates and counterplates (AE and BF).

The rollbar / tension rod assembly may be reinforced by two diagonals AF and BE. If the floor is not sufficiently resistant, these rods must be bolted onto the chassis. The plates and counterplates used above must have a surface area of 200 cm² and a minimum thickness of 3 mm, and be fixed by 4 bolts of 12 mm diameter.

6P5 CHASSIS

Truck's chassis must be freely available in retail shops and be produced at a minimum of 200 copies; it is also allowed to replace production model by any other model from the same range or even to build your own chassis respecting the following dimensions:



The two main chassis rails must be straight and parallel to the ground on their entire length.

Subfram under main chassis rails are strictly forbidden.

This chassis can be strengthened in order to rigidify the U-shape beam. It is possible to weld a metal plate to close the U to reinforce the chassis.

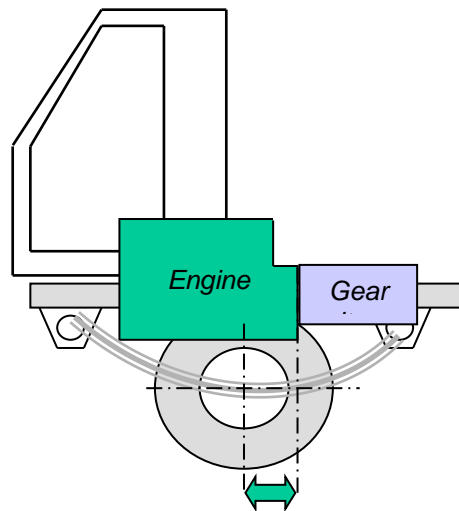
6P6 ENGINE

6P6.1 General

From 01/01/2013, no truck with a cylinder superior to 16.5 litres will be homologated.

The A.S.O. technical passport has a 5-year validity from the 01/01/2012. This validity only applies to trucks which have not undergone any modification during the 5-year period. Any truck having undergone modification will have to be submitted for a new validation of the A.S.O. technical passport by the A.S.O. authorised truck category responsible.

The engine must be directly fixed to the chassis but it is allowed to move it from its initial position. The maximum lengthwise distance is the following: 1,400 mm (longitudinal distance between the most advanced axle and the engine joint plan / gearbox).



6P6.2 Water coolant system

Water coolant radiators initial number must be kept; however, it is possible to modify these radiators' size and shape as well as associated fans and ducts' size and shape. Trucks must keep their exterior original aspect. It is forbidden to cut the chassis to allow a bigger radiator to pass.

6P6.3 Air induction system

The air filter(s) and tubing upstream of it (them) may be modified.

No part of the air induction system may project more than 300 mm beyond the lateral extremities or the top of the roof of the cab.

- No more than two air induction pipes may be fitted;
- The total cross-sectional area of the air induction pipes or ram air collector boxes must not exceed 1,000 cm².

6P6.4 Air restrictor: specifications

The engine must be fitted with a restrictor fixed to the compressor housing of the turbocharger.

All the air necessary for feeding the engine must pass through this restrictor, which must respect the following:

The maximum internal diameter of the restrictor is 74 mm, maintained for a minimum distance of 3 mm measured downstream of a plane perpendicular to the rotational axis and situated at a maximum of 100 mm upstream of a plane passing through the most upstream extremities of the wheel blades.

This diameter must comply with the above conditions, regardless of the temperature conditions.

The external diameter of the restrictor at its narrowest point must be less than 80 mm, and must be maintained over a distance of 5 mm to each side of the sonic collar.

The mounting of the restrictor onto the turbocharger must be carried out in such a way that two screws have to be entirely removed from the body of the compressor, or from the restrictor, in order to withdraw the restrictor from the compressor. Attachment by means of a needle screw is not authorised. For the installation of this restrictor, it is permitted to remove material from the compressor housing, and to add it, for the sole purpose of attaching the restrictor onto the compressor housing. The heads of the screws must be pierced so that they can be sealed.

The restrictor must be made from a single material and may be pierced solely for the purpose of mounting and sealing, which must be carried out between the mounting screws, between the restrictor (or the restrictor/compressor housing attachment), the compressor housing (or the housing/flange attachment and the turbine housing (or the housing/flange attachment).

In the case of a 2-parallel supercharger engine, each supercharger is limited by a 53 mm maximum interior diameter restrictor, and a 58 mm exterior maximum diameter restrictor, under the conditions specified above.

6P6.4 Oil catch tank

All engine breathers venting to the atmosphere must lead into a catch tank, arranged in such a way as to prevent oil from spilling onto the ground. If a single catch tank is used, it must have a volume of at least 4 litres. It is permitted to use multiple tanks, but each tank must be at least 2 litres. Tanks may be made of any material, but it must be possible to view the contents of the tank (e.g. a sight glass is required in a metal tank, and plastic tanks must be translucent).

All tanks must be capable of being readily emptied.

6P6.5 Fuel tank

Additional fuel tanks are free in respect of capacity. They must be of unmodified proprietary manufacture, of a type normally used in trucks.

Subject to the following requirements, fuel tank(s) position(s) is(are) free:

- If the fuel tank is situated outside the chassis, it will have to be fixed within the wheelbase; on the other hand, the position is free if the fuel tank is fixed in the transport box;
- Any fuel tank must be strongly fixed to the chassis. It must have an inferior protection against rocks and a lateral one to protect from shocks;
- Tank(s) must be firmly fixed to the chassis. They must have underbody protection against flying stones and lateral protection against impact.

6P7 GEARBOX

The gearbox must be a standard model used on trucks with a tonnage corresponding to the vehicle. It is strictly forbidden to modify the gearbox standard functions.



The gearbox must be placed side by side to the engine except if the standard model is different. In that case, dimensions will have to remain the same as the standard ones and the geartrain group (engine, box, bridge) will have to be standard and come from the same Vehicle (ex: Tatra).

6P8 AXLES

Axles' brand is free. The model must be from a commercialised model with a minimum batch of 200 produced units. It is allowed to modify the axle in order to adapt the different parts linked to competition use.

Also, it is allowed to fit 2 reinforcement bars in front of each axle. These bars' objective is to strengthen and guide the axle in case of big impact or landing off jumps.

6P9 PROPELLER SHAFT

For each longitudinal transmission shaft over 1 m long, a rollbar or a safety loop made from steel must be installed close to the front extremity.

6P10 SUSPENSIONS

T4.2 trucks must be equipped with parabolic type suspensions with rigid axles.

An exception will be made for rigid axles which do not have parabolic suspension as a standard feature. Only in this case, it will be possible to use suspensions identical to a standard model. A minimum of 200 commercialised units proof will need to be given by the manufacturer to A.S.O.

Trucks with independent wheels as a standard equipment will have to use original suspension system (pneumatic, hydraulic, etc.).

It is strictly forbidden to intervene on suspensions' settings from the cab. Only pneumatic settings adjusting the Vehicle's height will be allowed.

Any kind of active suspension is also prohibited.

6P11 SHOCK ABSORBERS

6P11.1 Shock Absorbers

Shock absorber type is free but with a 2 shock absorbers per wheel maximum limit.

The combined spring and shock absorbers are also authorized; the shock absorbers' conditions apply.

It is strictly forbidden to adjust shock absorbers from driving post. Any kind of adjustment while the truck is moving is strictly prohibited.

If hydraulic shock absorbers units are used, there must be no interconnection between the circuits.

The shock absorbers supports are free on condition that they have no other function than that of support.

The use of any kind of restraining stops is allowed. It is not possible to change these stops settings from the driving post or while the vehicle is in movement.

A maximum of one hydraulic stop and three elastic stops are authorised by wheel.

It is allowed to fit a direction shock absorber.

6P11.2 Travel Limitation

Suspension travel limitation straps must be installed.

The suspension travel is limited to 300 mm.

6P11.3 Control

Shock absorbers control measures are currently under study; they will soon be announced through official means and will apply during checks taking place before and during the 2013 Dakar.

6P12 STABILISER BAR

Stabiliser bars are allowed on all the truck's axles.

It is allowed to fix one shock absorber on the stabiliser bar. It is allowed to have an on/off button within the cab.

6P13 BRAKING SYSTEM

The braking system must be homologated for a road use.

Annual technical control and manufacturer's card will validate the braking system.

Brake cooling is permitted using ducted air only.

Cooling ducts must be fed by air intakes (one per wheel).

If the vehicle is originally fitted with a drum brake, it will be allowed to change this system to fit a disc brake system to provide higher safety. This system must be issued from an industrial production and being homologated for road use.

6P14 WHEELS AND TYRES

6P14.1 Wheel rims and spacers

6P14.1.1 General

Wheel rims and spacers must come from an industrial brand, not modified and must be made so to ensure that no wheel or tyre part touches any vehicle point in case of extreme conditions of suspension or direction movement.

Wheels' screw nuts and studs must be adapted to the rims used, to guarantee an adequate fixation resistance. Screw nuts must be from an industrial brand and not be modified.

Only ferrous material or aluminium alloy is authorised.

It is prohibited to fit any spacers or adaptors between the wheels and the hub.

6P14.1.2 Specificities

The weight of wheels in aluminium alloy must not be less than 35 kg.

The total diameter of the wheel when mounted and inflated to 5 bars must not exceed 1,300 mm.



The diameter must be measured on the new tyre specified by the Manufacturer. Wheels dismountable in several parts are forbidden (except flat dismountable rim of the "metallic beadlock" type).

R22.5 dimensions' wheels are allowed.

6P14.2 Tyres

Any tyre which the scrutineers consider being dangerous or in breach of the Regulations, for one reason or another, shall be refused.

All tyres fitted to the vehicle must have a tread depth complying with relevant national legal requirements for the duration of the event.

Hand grooved tyres are not permitted. Tyres fitted must be available through normal retail outlets for all-weather use on roads and/or tracks.

Roll band special tread compounds and/or patterns are not allowed, nor are any externally applied chemical compounds which may affect tyre grip.

All tyres used must be to E.E.C. Type Approval standard (E.E.C. Regulations 54) or equivalent. Retreated tyres are forbidden.

Remoulded tyres are forbidden.

No carcass may have undergone serious repairs.

6P14.3 Spare Wheel / Tyre

A minimum of 2 wheels or 2 tyres, depending on the type of wheels used, are compulsory.

6P14.4 Inflating / Deflating

The use of a system for inflating / deflating the tyres while the vehicle is in motion is authorised.

6P14.5 Wheel track and vehicle width

The combination of axles and wheels / tyres fitted must not cause the vehicle width to exceed 2,550 mm.

6P15 LIGHTING SYSTEM

All lamps required for normal legal road use must be functional at all times and must not be concealed.

The number of headlamps is limited to 8. They must be fitted in accordance with the International Road Traffic Convention.

6P16 VEHICLE WEIGHT

Regarding homologations, minimum allowed weight for over 10 litres cylinder vehicle is 8,500 kg.

For trucks using an engine under 10 liters cylinder, minimum allowed weight is 7000 kg except when standard model weights less than 7,000 kg. In that case, road homologation form will be taken as the reference.

These weights will be measured for trucks ready to be used, emptied of any material, without people and emptied of fuel. These limits are fixed for the vehicle having its 2 spare wheels.

APPENDICES



APPENDIX 1 – CANDIDATE FILES - ENTRIES

1.1 CANDIDATE FILE – ENTRY FEE

1) An application file (conditions and entry fees) can be downloaded from the www.dakar.com website from 15th May 2012. The application must be entered online on a website available from www.dakar.com. The application files must be returned to the Competitors' Department before 31st October 2012.

2) Payment deadlines must be followed. Any lateness will imply an augmentation of 10% of the fee.

3) The Organiser reserves the right to cancel registrations that have not been paid in full by 31st October 2012.

4) For any payment missed by the 31st October 2012, the Organiser reserves the right to forbid the participation to the Rally for the 5 next years.

1.2 CANCELLATION AND REQUEST FOR REFUNDS

1) So as to avoid any ambiguity, cancellation of entries and requests for refunds must be sent by registered post. Cancellations and requests for refunds must be sent to:

A.S.O. – Dakar Competitors' Department
253, Quai de la Bataille de Stalingrad – Immeuble Panorama B
92137 Issy les Moulineaux cedex - France

2) Refunds will be made by 31st March 2013 at the latest.

3) For all cancellation of entries, refunds will be made in the following manner:

- in the case of cancellations made before 1st September 2012 = €1,500 administration costs retained;
- in the case of cancellations after 1st September 2012 = 100% of the amount owed retained.

4) In the case of a serious problem, justified by medical opinion (original document), the partial repayment of monies paid can only be made if requested by registered letter. €3,000 will be retained.

APPENDIX 2 – BOAT – PLANE – ACCOMMODATION - TRIPS

2.1 BOAT

Shipment fees are included in the Vehicle entry fee.

Cars must measure no more than 2.00 m in height (rack included). Car over 2,00 m high will have to pay the 4x4 truck Vehicle fee -€4,400.

Trucks must measure no more than 4.00 m in height (rack included): official height of bridges, electrical lines, etc. in Peru, Argentina and Chili.

Trucks whose length is between 9 and 11 metres must pay a supplement of €1,000, those with a length of between 11 and 13 metres a supplement of €1,500. Please contact the Organisers concerning Vehicles over these lengths.

2.2 LIABILITY AND MARITIME INSURANCE

When crossing waterways, from the moment the Vehicle is embarked at the port of Le Havre or San Antonio (Chile), Public Liability cover taken out by the Organiser with AXA France is not effective.

Under maritime legislation and international conventions, the carrier's public liability insurance offers minimal cover for damages and loss of goods (Brussels Convention 1924 amended by Protocols in 1968 and 1979).

Therefore, during the Parc Ferme on the ports of Le Havre (France), Lima (Peru) and San Antonio (Chile) and during the maritime transport, the carrier's responsibility, and therefore the responsibility of the Organiser or of V.S.O., is strictly limited to legal terms (act dated 18 June 1966 and decree dated 31st December 1966) regarding the maritime carriage of Vehicles, which stipulate in particular the limits of reimbursement for damage to goods:

- 2 SDR (Special Drawing Right) per kilo or 666.67 SDR per package or unit (rate of 1 SDR as at 17/05/2012 = €0,8323),
- maximum indemnity per Vehicle transported: €4,600 following loss or damage.

It is therefore highly recommended that additional insurance be arranged to cover damage to Vehicles suffered during parking in Parc Ferme in Le Havre, in Callao and in San Antonio. This insurance is not mandatory but highly recommended, and may, if required, be arranged with the Sport Events Department of Gras Savoye whose contact details, offer and website address can be found on the Rally website at www.dakar.com, or be arranged with the insurance agent of your choice (submission possible by mail before the boat departure set up by the Organiser, and at the latest on 10th November 2012).

2.3 AIR FLIGHT - ACCOMMODATION – TRAVEL SERVICES

For the 2013 Dakar, our travel agency VSO offers a certain number of packages created around the event, including one for the "Rest Day" open to mechanics, sponsors, family and friends of Competitors.

Hereafter are the different proposed programs which you will find on the website:

- "The Big Departure" (Peru), the Dakar on the first kilometres of the Rally, from 4th to 7th January 2013
- "The Dakar in Paracas" (Peru), a total immersion in the big Peruvian spaces, from 5th to 8th January 2013
- "The Dakar and the Andes crossing (Argentina), the discovery of the Andean world, from 9th to 13th January 2013
- "The Rest Day" (Argentina), *the organisational backstage*, from 10th to 14th January 2013
- "Copiapo's dunes" (Chile), at the very heart of the Event and the human challenge, from 16th to 20th January 2013

2) As on previous years, V.S.O. also proposes different services to Competitors and Assistance in order to prepare your Rally the best way possible:

- accommodation at the start and finish and for the rest day;
- flights from Europe to Lima and return flights from Santiago, with the possibility to modify the return without cost, according to the availability;
- the V.S.O. assistance in the Competitors' tent on every Stage of the Rally to reorganise your returns in case of withdrawal, and to assist you for any kind of service.

Places being limited on these flights and for the packages, we recommend you to book now.

The V.S.O. agency is also at your disposal to set up any kind of specific request and to help you

through your organisation for the 2013 Dakar.

You can find all the necessary information on the website www.dakar.com within the "Competitors" section.

3) Don't hesitate to contact:

V.S.O.
253, Quai de la Bataille de Stalingrad – 92137 Issy-les-Moulineaux cedex
Tel. : 33 (0) 1 41 33 14 85 – Fax : 33 0 (1) 41 33 14 86
e-mail: vso@aso.fr

APPENDIX 3 – SAFETY EQUIPMENT & GPS

All Safety and Navigation suppliers will be present:

- every morning at the start from the Bivouac,
- permanently at the Safety Center, close to the « Malles Motos ».

for advice, repairs... and, in the case that you retire, to collect your equipment.

3.1 SARSAT DISTRESS BEACON

1) The rental of the obligatory Sarsat distress beacon is included in the global Vehicle tariff. You will receive the distress beacon at administrative checks from our service provider, TDCOM.

2) You will be charged a deposit by TDCOM.

You will have to pay this deposit when booking your Sarsat distress beacon before the delivery of the instrument in Le Havre.

Your deposit covers the Sarsat distress beacon for the amount of €750 including taxes, guaranteeing the return of material in good working condition.

3) In the case where a new Sarsat distress beacon is requested during the Rally (following problems with the first), a new deposit will be requested directly by our supplier. In case of damage to the Sarsat distress beacon, an estimate will be made by the factory and an invoice issued by TDCOM.

In the case where the Sarsat distress beacon is used, the costs of replacing the battery and a check of the Sarsat distress beacon will be invoiced at €280 including taxes for car distress beacons.

4) The Sarsat distress beacon must be returned at the end of the rally to TDCOM, who will ensure recuperation on the ground. The deposit of any Sarsat distress beacon not returned to TDCOM before 25 February 2013 in working order will be cashed. After this date, any disputes will be dealt with directly by the supplier who will invoice €76 per week, for additional hire.

3.2 IRITRACK (RACE)

1) The hire of an Iritrack is compulsory and included in the Vehicle entry fee. It will be issued to you by the supplier TDCOM, before boarding in Le Havre for Competitors embarking on board the ferry set up by the Organiser or in Lima on 2nd January 2013 from 2.00pm for Competitors not having embarked on board the ferry set up by the Organiser.

The installation kit, including the mounting bracket, cables and aerials, will be sent by courier on receipt of your order form and payment.

This kit must be installed before technical scrutineering and power must come directly from the battery.

On confirmation of your entry, the Organiser will send you information regarding this equipment.

2) The use of this equipment is compulsory (on pain of the start being refused) as is the maintaining of the equipment in working order, throughout the Event, and over the whole of the route, including Road Sections.

3) You will be charged a deposit by TDCOM.

You will have to pay this deposit when booking your Iritrack before the delivery of the instrument in Le Havre.



Your deposit covers the Iritrack for the amount of €1,500 including taxes, guaranteeing the return of material in good working condition.

4) In the case where a new Iritrack is requested during the Rally (following problems or loss of the first), a new deposit will be requested directly by the supplier.

In case of damage to rented equipment an invoice will be issued by TDCOM.

5) The Iritrack must be returned at the end of the Rally to TDCOM, who will ensure recuperation on the ground. The deposit of any Iritrack not returned to TDCOM before 25th February 2013 in working order will be cashed. After this date, any disputes will be dealt with directly by the supplier who will invoice €76 per week (taxes not included), for additional hire.

3.3 TELEPHONE – SAFETY EQUIPMENT

All safety and survival equipment as detailed in Article 47P1 can be obtained, like satellite telephones, from TDCOM.

For further information concerning the three pieces of equipment please contact:

TDCOM – Rallye Raid Department
114, Avenue d'Alfortville – 94600 CHOISY-LE-ROI
Tel.: +33(0)1 48 84 34 00 – Fax: +33(0) 1 48 52 53 54
rallye@tdcom.fr

3.4 GPS EQUIPMENT

1) To ensure an equal chance for all, the Organiser will supply all Competitors with a specific single type of GPS. The functions of this equipment have been intentionally restricted in order to preserve an element of navigation in the desert.

The GPS Unik II will have the Sentinel function integrated.

It is compulsory to carry this equipment on board (otherwise the start will be refused) and to keep it switched on, throughout the Event, and that over the entire route, including Road Sections.

A maximum of two GPS Unik II (homologated A.S.O.) might be authorised by Vehicle, depending on the supplier's availabilities.

2.a) The rental of obligatory GPS is included in the global Vehicle fee.

It includes:

- 1 "colour" GPS for CAR-TRUCK categories Vehicles;
- 1 "colour" GPS for BIKE-QUAD categories Vehicles.

The instruments will be delivered by the supplier, ERTF, before boarding, in Le Havre.

2.b) The fitting of the GPS requires an installation kit, not included in the Vehicle fee, that the Competitor must procure directly from ERTF. It must be installed before scrutineering.

NB: For Car and Truck Competitors, Vehicles will be equipped with the Unik II (GPS with Sentinel integrated). Competitors must ensure the fitting of the accessories necessary to install the GPS Unik II.

Competitors will be required to ensure the following:

- the mechanical and electrical installation, with a continuous power supply of between 9 and 30 volts, regulated and protected by a **3 amp fuse for cars – trucks** and a 2 amp fuse for bikes and quads.
- the mechanical mounting must be flexible and include the supplied silent blocks.

3) Compass heading repeaters and/or speed or distance trips, other than the model homologated by the Organiser, are prohibited, as are all other accessories not homologated and connectable to the GPS. This optional material can be purchased from the suppliers.

Upon confirmation of entry, the Organiser will forward to you an information sheet concerning this equipment.

4) You will be charged a deposit by ERTF.

You will have to pay this deposit when booking your GPS before the delivery of the instrument in Le Havre.

Your deposit covers the GPS for the amount of €1,700 including taxes, guaranteeing the return of material in good working condition

5) In the case where a new GPS is requested during the rally (following problems or loss of the first), a new deposit will be requested directly by the supplier.

In case of damage to rented equipment an invoice will be issued by ERTF.

6) The GPS must be returned at the end of the Rally to ERTF, who will ensure recuperation on the ground. The deposit of any GPS not returned to ERTF before 25th February 2013 in working order will be cashed. After this date, any disputes will be dealt with directly by the supplier who will invoice €76 not including taxes per week, for additional hire.

7) For further information, contact:

Société ERTF COMPETITION
Parc Technologique de Soye - 56275 PLOEMEUR
Tel.: +33 (0)2 97 87 25 85 - Fax: +33 (0)2 97 37 59 21
competition@ertf.com

3.5 SENTINEL FUNCTION OF GPS

1) So as to increase safety of Competitors during overtaking and to avoid accidents, the use of the Sentinel equipment is compulsory (on pain of being refused a start) throughout the whole event, over the whole route.

The compulsory hire of the Sentinel is included in the Vehicle entry fee. The GPS with Sentinel function will be delivered by our supplier, ERTF, before boarding in Le Havre.

On confirmation of your entry, A.S.O. will send you an information note concerning this equipment.

2) For all additional information please contact:

Société ERTF COMPETITION
Parc Technologique de Soye - 56275 PLOEMEUR
Tel.: +33 (0)2 97 87 25 85 - Fax : +33 (0)2 97 37 59 21
competition@ertf.com

3.6 TRIPY – FOR ASSISTANCE ONLY

1) A.S.O. will supply to all assistance and press Vehicles an electronic Road Book, a Tripy TRM-II-C. It will aid navigation for Drivers of assistance Vehicles and press cars.

The use of this equipment is compulsory (on pain of being refused a start) and it must be switched on and functioning throughout the Route.

2) The compulsory hiring of the Tripy electronic Road Book is included in both the assistance and press entry fees. It includes one electronic Tripy TRM-II-C Road Book.

The checking of the cables will be done in Le Havre for assistance and press vehicles departing from Europe and on 2nd, 3rd and 4th January for the other vehicles. The equipment will be delivered at the administrative checks, in Lima by the supplier TRIPY.

It is your responsibility, before arriving at Le Havre (for assistance and press vehicles embarking on board the ferry set up by the Organiser) or in Lima (for the other vehicles) to:

- fit a RAM MOUNT type round mounting point;
- fit a GPS antenna (a magnetic antenna that fits to the roof of the vehicle and antenna cable returning to the place where the mount had been installed),
- fit a power cable – between 9 and 30 volts DC – linked directly to the battery (and not after the vehicle's ignition key, neither after a circuit breaker).

So as to be able to carry out the installation of these three accessories before arriving in Le Havre or in Lima, you will have to order the necessary parts from TRIPY who will send them along with



detailed fitting instructions. These accessories must be paid for (the details of our supplier TRIPY are below).

3) You will be charged a deposit by TRIPY.

You will have to pay this deposit when booking your Tripy. The material will not be given out if the deposit has not been paid.

Your deposit covers the electronic Road Book for the amount of €1,000 including taxes, guaranteeing the return of material in Lima, in good working condition, and on time.

4) In the case where you request a new Tripy electronic Road Book (following lose or problems with the original) a new deposit must be made directly to the supplier. In case of damage to equipment hired, an invoice will be sent by Tripy.

5) Electronic Tripy Road Books must be returned to Tripy at the end of the Rally at the assistance park in Santiago on 19th January 2013. A receipt will be given in exchange. Failure to return the Tripy electronic Road Book at this time will result in the retaining of the deposit.

6) For all further information please contact:

TRIPY S.A.
Faubourg de Bruxelles, 320 – B6041 Gosselies – Belgique
Tel.: +32 (0)71 34 74 90 - Fax: +32 (0)71 34 73 99
dakar@tripy.be

3.7 RADIO VHF – FOR ASSISTANCE CREWS ONLY

1) The hire of the VHF radio is possible for assistance vehicles, only from the supplier TDCOM.

2) The radio will be delivered at the administrative checks by TDCOM.

The installation kit, comprising of mounting brackets (radio and microphone), a magnetic 150 MHz aerial, a power supply cable running off the cigarette lighter and an open power cable will be sent by courier as soon as the order and payment have been received.

The supplier will send you an information note on this equipment.

3) For all additional information please contact:

TDCOM – Rally Raid Department
114, Avenue d'Alfortville – 94600 CHOISY-LE-ROI
Tel.: +33(0)1 48 84 34 00 – Fax: +33(0) 1 48 52 53 54
rallye@tdcom.fr

APPENDIX 4 – DAKAR BRANDS - TRADEMARKS

The Organiser reminds owning, on an exclusive basis, the exploitation of the nominative, figurative and/or half-figurative "Dakar" brands.

The commercial exploitation by a third party, even if it is only by referencing, of the intangible attributes of an event, can only be authorised by the written and express consent of the Organiser who grants the use, upon conditions, to its sponsors.

APPENDIX 5 – AUDIO VISUAL COVERAGE

1) In order to permit the widest possible dissemination and promotion of the DAKAR, any and all persons taking part in the DAKAR for any reason (hereinafter collectively referred to as the Competitors) acknowledge and agree that their participation in the competition grants to the organizers and assignees the irrevocable right to reproduce and display, without any compensation whatsoever, their name, likeness, voice, biography and history, and more generally, their sports performance in connection with the DAKAR, as well as the trade name(s), trade mark(s), brand(s) of their Vehicle parts manufacturers, constructors and sponsors, in any and all forms, on any media (whether now existing or hereafter developed) and in any format, for all kinds of world-wide use, including advertising and/or commercial use without any restrictions whatsoever except for those mentioned hereinafter, and for the entire duration of the protection currently granted for such use by legal or statutory provisions, judicial and/or arbitral decisions of all countries, as well as international conventions (present and future), including eventual extensions that could be made to this duration.

However, when the Organiser authorises a third party to use pictures of the competition for advertising or promotional purposes, the third party will not be allowed to use the name, voice, likeness, voice, biography, history or sports performance of a Competitor or the trade name, trade mark of his sponsor, Vehicle parts manufacturer or constructor, with a view to a direct or indirect association between the Competitor, the trade name, the trade mark of his sponsor or Vehicle parts manufacturer, and the product, service, trade name or trade mark of the said third party without the express consent of the Competitor, sponsor or vehicle parts manufacturer.

2) Competitors and accompanying persons may not be allowed to film pictures of the DAKAR, whatever the means used and the purpose for which they are intend to film, without the prior written consent of the organizer. In this connection, written requests must be sent at the latest by the 1st of December preceding the start of the competition, to the following address:

A.S.O. – TV Department
253, Quai de la Bataille de Stalingrad – 92137 Issy-les-Moulineaux cedex
Tel.: +33.(0)1 41 33 14 10 - asotv@aso.fr

APPENDIX 6 – ONBOARD CAMERAS

TECHNICAL FILE

ONBOARD CAMERAS "RECORD" CAR

Product	Qty	Unit Weight (Kg)	Total Weight (Kg)	Unit Size L x w x h (mm)	Power requirements (Volt x Amp = Watt)
Camera + cable	3	0.400	1.200	60 x 40 x 30	12 x 0.2 x 3 = 7.2
Camera mount	3	0.300	0.900	-	
Recorder + Tsumiki trigger	1	0.400	0.600	130 x 95 x 35	12 x 0.8 = 9.6
GPB Sequencer	1	1.720	1.720	250 x 200 x 90	12 x 0.6 = 7.2
Battery	1	0.820	0.820	140 x 100 x 70	-
GPB Sequencer mount format	1	-	-	250 x 200 x 90	-
Recorder + trigger mount format	1	-	-	140 x 100 x 45	-
Total			5.240		12 x 2 .2 = 26.4

Summary "Car Record" :

3 cameras with their mounts, un mount integrating 1 GPB sequencer and another mount integrating a recorder and a Tsumiki trigger

Total weight (approx.) : 5.300 Kg

Power supply : 12 Volt / 3 Amp. / 36 Watt

APPENDIX 7 - DAKAR SERVICE CENTER

Further than the values shared between the sponsors and the Dakar, one of the sponsors' top priorities is to accompany the Competitors in their adventure!

Find all the sponsors services within the Dakar Service Centre set up in every Bivouac.

The December newsletter will give you all the details concerning the services offered by the sponsors on the Dakar Service Center 2013.



ASSISTANCE REGULATIONS

1. SPORTING REGULATIONS - ASSISTANCE

Foreword:

The Assistant Clerk of the Race responsible for Service Vehicles, named by the Organisation Committee will be in charge, with his team, of the Assistance Regulations application and respect. He will notably be able to apply penalties with regards to the noticed infringements, by any available mean.

He will communicate to the Organiser and/or to the Sporting Direction the infringements in order to apply the appropriate sporting penalties of these Regulations when necessary.

Terminology: refer to the 2013 Dakar Rally Specific Regulations.

ART 1 - ELIGIBLE ASSISTANCE VEHICLES

The nature of the terrain, the weather risks, the dangerous nature of certain roads and tracks and the crossing of the Andes lead to regulate the conditions of entry for assistance category Vehicles.

1) ARE ADMISSIBLE

a) 4x4 Light Vehicle category (licence B)

- **4x4 Production Vehicles** petrol or diesel without any special preparation.
- **4x4 equipped Vehicles:** equipped with rollbars, bucket-seat and harness. Only these 4x4 equipped Vehicles will be allowed to go to the Selective Section in case of exceptional circumstances and upon the Organiser's approval.

Light 4x4 Vehicles will not, in any case, be over 2.00 meters high (including the roof track), on pain of having to pay the 4x4 truck assistance Vehicle fee costing €3,500.

b) Camping-cars and equipped bus

- Standard **camping-cars**, smaller than 7.5 meters lengthwise (example: Sailer 669, Ford Transit, Mercedes Rapido, etc.) may be accepted, even with 2 wheel-drive, after having submitted an application file with the Vehicle picture (visible license plate) and a copy of the registration card to the A.S.O. Sporting Department. The Vehicles will have to remain in the 'living space' setting and will not be exclusively dedicated to transporting parts.
- **Equipped Bus:** a €10,000 additional fee (on top of the 4x4 truck assistance fee) will apply for any equipped bus submission, upon A.S.O. Sporting Department's approval, after presenting the Vehicle's pictures (visible license plate) and a copy of the registration card. The Vehicles will have to remain in the 'living space' setting and will not be exclusively dedicated to transporting parts. **It will not, in any case, embark on board of the ferry put in place by the Organiser, departing from Le Havre and returning from Santiago.**

Any specific request will have to be sent to the A.S.O. Sporting Department before September 30th, 2012.

For both categories, the Organiser cannot be held responsible if these vehicles' types cannot access some of the Bivouacs.

These vehicles' types will not be able to park on the created roads inside the Bivouacs.

It is strictly forbidden for a Competitor to park or to ask someone to park his camping-car(s), equipped bus or assimilated, not accredited by the Organiser, within a 2km minimum radius around the Bivouacs. Only accredited vehicles will be allowed within that area, on pain of a €700 penalty for infringement. Any repeated offence may lead to penalties going up to and including disqualification.

c) Service trucks

- **T5 category:** Cross-country standard trucks over 3.5 tons, in accordance with T4 truck Regulations, even if they do not benefit from the FIA homologation. Only these vehicles will be allowed to have access to the Selective track after receiving the Organiser's approval.
- **T5.1 category:** standard cargo trucks ("construction" type) in 6x4 or 8x4 versions. The maximum authorised load weight is respectively 26 and 32 tons.

Caution, models with only one engine axle will not be authorised (4x2, 6x2, 8x2). T5.1 trucks length must comply with the European code, therefore, the maximum length authorised is 12 metres.

These vehicles will not, in any case, be over 4.00 metres high (including the roof track). Caution: the high limit is 4.00 metres in Peru, Argentine and Chili with regards to bridges, electrical lines, etc.

Trucks which length is between 9 and 11 metres will have to pay a €1,000 additional fee, these which length is between 11 and 13 metres will have to pay a €1,500 additional fee. Over 13 metres, please contact the Organiser.

d) Carrying people

Carrying accredited people is authorised in light 4x4 vehicles (license B) respecting the registration card prescriptions and for a 9-people maximum capacity.

e) Platform trailers

For logistical restrictions, particularly linked to the decks filling capacities (with different highs) of the ferry departing from Le Havre and returning from Santiago, the Organiser had to apply very strict measures:

- **Platform trailers for cars and bikes:** They will be subject to the submission of an application file with pictures sent to the A.S.O. Sporting Department. Their maximum dimensions will not exceed H: 1m, L: 6m, l: 2.5m. **The ones loaded on the Organiser's boat departing from Le Havre will have to be emptied of any vehicle and materials and will be dissociated from the vehicle towing them.**
- **Platform trailers for trucks:** They will be subject to the submission of an application file with pictures sent to the A.S.O. Sporting Department. Their maximum dimensions will not exceed H: 1m, L: 9m, l: 2.5m. Any trailer over 6 metres will have to pay a €1,000 additional fee. Double hinge trailers (dolby) are forbidden. The truck trailers loaded in Le Havre on the Organiser's boat will be allowed to be hooked to the vehicle towing them, and will also be allowed to bear another vehicle.

Any specific request will have to be sent to the A.S.O. Sporting Department before September 30th, 2012.

f) Semi-trailers

Semi-trailers can be accepted upon certain conditions after submitting an application file and upon approval of the A.S.O. Sporting Department. Files will have to be sent before September 30th, 2012. This type of vehicle will follow the Rally through the assistance itinerary and will not have access to the bivouac.

2) ARE FORBIDDEN

- a) Truck trailers;
- b) Closed trailers;
- c) 2 wheel-drive vehicles (except camping-cars, mobile-homes and equipped bus);
- d) Covered cells.

3) Both during the Event administrative checks and at Le Havre embarkation, the Organiser reserves the right to refuse any vehicle not being adapted or not corresponding to the declared one. After going through scrutineering in Lima, it will not be possible to change vehicle.

ART 2 - CREWS

1) Admissible, upon invitation, all persons over 18 years of age.

2) Crews may be composed of:

- **Light Vehicles (licence B):** 1 to 9 persons, depending the type of Vehicle entered. Only 4x4s where the registration document stipulates the possibility to carry 4 people will be allowed to transport 4 people.
- **T5 Category:** 2 to 5 persons, if the type approval of the truck allows for 5 people and if this is stated on the driving documents (4 doors for use of 5 people).
- **T5.1 Category:** 2 people as standard, following the type approval of the truck.
- **Camping car, mobile home:** 2 to 3 people, in the respect of the registration documents.
- **People carrier:** the maximum number of people may not exceed 9.

For all request for extra passengers, a formal request must be made to the Organisers who may, according to conditions, accept or refuse an additional person.

3) FIA licences are not necessary for assistance Crews.

In the truck category, it is authorised to have only one holder of the heavy goods' driving licence on board.

4) No changes to assistance Crew members will be allowed once scrutineering has been passed, under pain of a €500 fine, per infringement, per assistance Crew member; except for those persons entered as Team Managers, or if authorised, exceptionally by the Organisers.

5) If an assistance Crew member leaves their Vehicle due to force majeure, the vehicle may continue providing the Organisers are informed. In no case can the Crew member be replaced. During the Event, under exceptional circumstances and following authorisation from the Organisers,



a person may continue alone aboard his vehicle.

6) All infractions will incur the Disqualification of the assistance vehicle.

ART 3 - BRIEFING

A general Briefing (race and assistance) will be held in Lima on 4th January 2013 09.00pm. Will be detailed all the information regarding the safety and the route. This information will be dated and signed by the Clerk of the Race, and will be posted on the Official Notice Board.

The presence of at least one member of each assistance Crew is compulsory, on pain of a penalty of €500.

ART 4 - ENTRIES

Conforming to the current Specific Regulations: Article 14P and Appendix 1.

All "assistance" entries must be attached to a Competitor in the race to be accepted.

ART 5 - IDENTIFICATION

1) An electronic chip ("RFID" type) will be fixed on every assistance vehicle. It will be checked at every Bivouac entry and exit; the Crew members' chips will also be checked.

2) An identification bracelet, equipped with a "RFID" type electronic chip will be attributed to every assistance Crew member, during administrative checks in Lima. This bracelet is individual and cannot be exchanged and/or given at any given time. It will notably be used to access the Bivouac and the catering area and might be controlled at any given time.

The emergency phone number of the PC (+33 1 41 33 15 81) is written on the bracelet.

In the case of loss or damage, the participants will have to ask for a new one to the Competitor Department in exchange of the damaged one.

ART 6 - ATTRIBUTION OF NUMBERS AND START ORDER

6.1 ATTRIBUTION OF NUMBERS

Numbers will be attributed at the discretion of the Organising Committee.

6.2 START ORDER

During the Rally, starts will be free, except on some Stages where start times will be imposed and written on the time schedule handed out to the participants at the administrative checks in Lima. Start will be organised through the race number, a Time Card will be handed out to each Crew. Any Crew who does not respect this procedure will be penalised by a 2-hour immobilisation of the vehicle at the Bivouac entry.

ART 7 - CHECKING-IN / TRIPY

7.1 GENERAL

Assistance vehicles will be allowed to leave the race the day before the last Stage so as to arrive to Santiago in advance. This authorisation will be given by the Assistant Clerk of the Race responsible for the Service Vehicles, upon request.

7.2 TRIPY

1) All assistance vehicles will be equipped with a Tripy system combining an automatic Road Book and GPS.

The hire is included in the vehicle entry fee, except for vehicles passing from the race to assistance, which must pay a hire fee to the Organiser's supplier (Tripy), depending on the supplier's availability.

2) Checking procedure

Each evening, on arriving at the Bivouac, the information stored in the GPS will be transmitted by radio to a technician's computer. If irregularities are noted (Way Point, speed, etc.) the person carrying out the checks will ask the Crew to sign a form and 'on the spot' penalties will be given.

3) Throughout the duration of the Rally, Crews are responsible for the correct functioning of their GPS Tripy. The Tripy(s) must be switched on and connected permanently throughout the entire Stage.

4) All actions caused by Crews (loss, destruction, switching off, etc.) making it impossible to read the GPS and/or all attempts at fraud or manipulation noted will result in the following penalties:

- 1st infringement: fine of €300,
- 2nd infringement: fine of €500,
- 3rd infringement: immobilisation at the Start Time Control at the Bivouac until the start of the 1st truck (for an assistance car) or of the last truck (for an assistance truck) during the next Stage.
- 4th infringement: Disqualification.

5) In the case where either the speed or route are contested, the Crew must pay the amount of the fine as a guarantee to the Assistant Clerk of the Race responsible for Service Vehicles, before their Tripy is downloaded at the Safety Centre by a technician. If the checks prove negative the amount paid will be returned to the Crew.

ART 8 - ROAD BOOK / ITINERARY

8.1 ROAD BOOK

In addition to the Road Book integrated into the Tripy, Crews will receive a paper Road Book during administrative checks.

8.2 ITINERARY

1) The following of the itinerary, as described in the Road Book is compulsory. Vehicles must follow the entirety of each Stage on pain of Disqualification. They may not avoid a Stage and then return to the race, unless they make a specific request to the Assistant Clerk of the Race responsible for Service Vehicles.

2) Failure to respect the itinerary will incur the following penalties:

- 1st infringement: fine of €300,
- 2nd infringement: fine of €500,
- 3rd infringement: immobilisation at the Start Time Control at the Bivouac until the start of the 1st truck (for an assistance car) or of the last truck (for an assistance truck) during the next Stage.
- 4th infringement: Disqualification.

ART 9 - DRIVER'S CODE

9.1 GENERAL BEHAVIOUR

1) Vehicles and persons registered in the assistance category are obliged to behave respectfully on the road and towards:



- the population of the countries crossed;
- the other Competitors;
- the members of the Organisation.

2) All incivility noted will result in a penalty of €500.

Any repeated infringement will be judged by the Assistant Clerk of the Race responsible for Service Vehicles and will lead to penalties up to and including Disqualification.

3) It is forbidden to leave wheels and/or punctured or damaged tyres on the itinerary. Any Crew caught disobeying this rule will be penalised €1,000 per tyre or wheel. Repeat offending will result in penalties up to and including Disqualification.

9.2 ACCIDENT

Any Crew involved in an accident which results in physical harm or not will be subject to an investigation. Depending on the circumstances, penalties up to and including Disqualification may apply.

9.3 MANDATORY STEPS WITH REGARDS TO CUSTOMS FORMALITIES

1) PACKING LIST

- The Packing List is defined as the mandatory and official administrative document of the temporary importation of each Vehicle, whatever category it is registered in, race and assistance;
- The Packing List will be filled by the Competitor with the best of care and will include:
 - The vehicle references;
 - A detailed list of all the good and materials transported as well as their commercial value;
 - Any other kind of light vehicle on board the vehicle (bicycle, bike, quad, scooter, etc.).
- The transported goods must imperatively stay identical from the departure from Europe and/or the arrival in Peru, to the last border crossing AFTER the Event and conform to the Packing List.
- The Competitor must permanently keep this document with him; the Competitor must give a copy to the Rally customs declarant as well as to the Organiser during administrative checks, and/or at the embarkation in Le Havre;
- No border can be crossed without this document.

2) BORDERS' CROSSING

At each border, every race and assistance vehicle must, on top of the checks organised by the national customs, present itself to the checks set up by the custom declarant.

3) TRANSPORTATION OF SPARE PARTS

- In order to comply with customs requirements, and to ensure there are no problems in crossing borders, or when embarking: all borders must be crossed with the same spare parts in the Vehicle as declared at the outset, and detailed in the mandatory Packing List of each Vehicle. It is therefore strictly forbidden to sell or transfer any items being transported;
- In case of the transportation of a vehicle having withdrawn (car, bike, quad) inside the assistance vehicle, it will be **COMPULSORY** to advise the customs declarant and the Competitors Department so as to regulate the situation with the customs authorities.

- In addition to the risks incurred locally by failure to comply with the rules in force (fines, impoundment of Vehicle/passengers, etc.), any Crew found to have breached this rule will be fined €500 per item (e.g.: one tyre = €500). Any subsequent offence may incur penalties, including Disqualification.

4) STEPS TO FOLLOW WHEN WITHDRAWING

In case of an assistance vehicle's withdrawal, its Crew will ABSOLUTELY have to, on top of complying with the Article 15.2 requirements of the Assistance Regulations, tell as soon as possible the customs declarant as well as the Competitors Department about the vehicle state, its position and the steps taken to transport the vehicle back to: either San Antonio port (in Santiago, Chile) to embark in direction of Le Havre, either to the vehicle's origin country if it has not embarked in Le Havre on the Organiser's ferry.

5) SALE/TRANSFER OF VEHICLE

- If a Crew withdraws or at the end of the Rally, it is STRICTLY FORBIDDEN to transfer or to sell its vehicle in any of the countries crossed, knowing that the vehicle is under the temporary importation Regulations and is linked to its Packing List.
- In addition to the risks incurred locally by failure to comply with the rule in force (fines, impoundment of vehicle/passengers, etc.), any Crew found to have breached this rule will not be permitted to enter the Dakar Rally for 5 years.

6) LOSS OF VEHICLES (COMPLETE OR PARTIAL DESTRUCTION)

- If a vehicle is lost (accident, fire, destruction, etc.), the Competitor must make a statement to the police or customs. This statement must contain the facts of what happened, the registration number, the race number, and the contact details of the people involved, etc. A copy of this statement must be provided to the Competitors Department and to the customs declarant.
- The vehicle must be returned immediately to the port of San Antonio in Santiago (Chile) and at the latest on 21st January 2013 at 08.00am, where the local authorities will decide, depending on the state of the vehicle, whether it should be re-exported, destroyed, or whether cancellation of its temporary import is justified. Any additional repatriation costs must be paid by the Competitor.
- In addition to the risks incurred locally by failure to comply with the rules in force (fines, impoundment of vehicle/passengers, etc.), any Crew found to have breached this rule will be fined €1,500. In the case of damaged vehicles that came on the road, the Competitors must approach the Organisers to find out what to do.

7) CROSSING THE ARGENTINE / CHILE AND CHILE / PERU BORDERS

- For health reasons, it is strictly forbidden to take the following items from Peru to Chile, from Chile to Argentina and from Argentina to Chile (SAG, act n°18755 1989, modified in 1994, act n°19283 – SENASA resolution n°295 in 1999 and resolution n°816 in 2002). Vehicles will be searched prior to crossing the border.

Prohibited products:

- Fruit, vegetables, herbs, plants, tubers, bulbs, grains, hay, twigs, soil, cut flowers, handicrafts, wood and other vegetable products, pesticides and fertilisers;
- Animals, birds, bees, honey, cheese, milk, meat and dairy or meat products, embryos, seeds, veterinary medicines (vaccines, diagnostic kits, antibiotics, medicated shampoo, etc) or any other animal products or any animal food;
- Protected flora and fauna and/or by-products derived from species under threat of extinction, including threatened species of flora and fauna under the Convention on International Trade in Endangered Species (CITES).

- In addition to the risks incurred locally by failure to comply with the rules in force (fines, impoundment of vehicle/passengers, etc.), any Crew found to have breached this rule will be penalised with a fine of €500 and may be disqualified.

ART 10 - TRAFFIC - SPEED

- 1) Throughout the entire Event, Crews must strictly observe the traffic laws of the countries crossed and especially respect local speed limits when they are less than the maximum speed allowed.
- 2) It is for Crews to adapt their speed to population and traffic. Radar controls may be carried out on the itinerary by the Organisation and the local police.
- 3) All vehicles not respecting the highway code of the countries crossed, driven at excessive speed or behaving in a dangerous manner and which are subject to a police or Organisers' report risk penalties up to Disqualification.

10.1 SPEED – SPEED CONTROL ZONES

- 1) There will be some Speed Control Zones on the itinerary, in villages, on roads or tracks, etc. They will be indicated on the Road Book and controlled by the Tripy. When driving through these zones, assistance vehicles will have to respect the speed mentioned on the Road Book.
- 2) Over speeding will be displayed on the Tripy screen as cumulative distances of over speeding depending on different slots.
- 3) All over speeding equal or over 1kph and under or equal to 20 kph will lead to the following penalties:
 - For any over speeding cumulative distance inferior to 500 m:
€200 fine,
 - For any over speeding cumulative distance between 500 m and 1,000 m :
Sealing of the vehicle at the Bivouac + €400 fine;
 - For any over speeding cumulative distance over 1,000 m:
Disqualification of the Vehicle and its Crew.

All over speeding superior to 21 kph will lead to the following penalties:

- For any over speeding cumulative distance inferior to 500 m:
Sealing of the vehicle until TC at the End of Stage of the next Bivouac + €400,

For any over speeding cumulative distance superior to 500 m:
Disqualification.

10.2 MAXIMUM SPEED – SPEED CONTROL ZONE

1) In Peru the maximum speed is 80 kph for trucks and 100 kph for cars; in Argentina and Chile, the maximum speed allowed is limited to 90 kph for trucks, 110 kph for cars;. This speed will be checked by the Tripy GPS. If the speed is exceeded a sign will appear on the GPS screen, accompanied by a noise warning.
The GPS will be checked at the finish of the Stage, the information being automatically transmitted by radio signal. In the case of an infraction, a controller will note the infraction and request that a member of the Crew counter signs the note.

2) Penalties

If the speed limit is exceeded an pulse signal will be recorded by the GPS.

All excess speed will result in the following penalties:

- From 1 to 20 kph over the speed limit:
 - If the cumulative distance is less than 1,000 m:
€200 fine;

- If the cumulative distance is between 1,000 and 2,000 m:
€400 fine;
- If the cumulative distance is over 2,000 m:
€600 fine.
- From 21 to 40 kph over the speed limit:
 - If the cumulative distance is less than 1,000 m:
€400 fine;
 - If the cumulative distance is between 1,000 and 2,000 m:
€600 fine;
 - If the cumulative distance is over 2,000 m:
Sealing of the Vehicle until TC at the End of Stage of the next Bivouac + €800.
- More than 41 kph over the speed limit:
 - If the cumulative distance is less than 500 m:
€800 fine;
 - If the cumulative distance is over 500 m:
Sealing of the Vehicle until TC at the End of Stage of the next Bivouac
+€1,000.

Fines must be paid within 48 hours on pain of the Vehicle being immobilised at the Bivouac.

10.3 SPEED BIVOUAC/STAGE TOWN

It is forbidden to drive at excessive speed and/or drive dangerously in the area of the Bivouac or in Stage towns, on pain of penalties up to and including Disqualification, to be decided upon by the Assistant Clerk of the Race responsible for Service Vehicles.

ART 11 - ASSISTANCE

11.1 GENERAL

1) See Article 31P of the Specific Regulations.

2) It is forbidden for assistance Vehicles to intervene on the day's Selective Section, on pain of Disqualification of the assisted Competitor. However they can intervene on Road Sections, only on sections of the itinerary shared with Competitors.

Vehicles and persons entered in the assistance category are not allowed to go on to the route of the Selective Section, except after the closure of the finish Time Control and after having informed the Organisers.

3) Vehicles and persons entered in the assistance category are not allowed to go on Public Zones, except if the latest feature on the assistance Road Book.

4) For safety reasons, people registered in the assistance category must inform the Organiser when they go back on the track to get a Competitor, on pain of Disqualification of both Vehicles.

5) Signposting of any kind is forbidden throughout the entire route.

6) For safety reasons, assistance Vehicles are not allowed to transport fuel (tolerance of 20 litres). All refuelling (of petrol) of a Competitor by an assistance Vehicle is forbidden, on pain of Disqualification of the assisted Competitor.

The filling of fuel tanks of T1 or T2 diesel Vehicles from a T5 truck may only be carried out in the Bivouac and only if the fuel tank of the T5 is situated outside the cargo and under the Vehicle.

7) All infractions of the assistance Regulations will result in penalties up to Disqualification of the assistance Vehicle and the race Vehicles concerned.

11.2 PENALTIES FOR FORBIDDEN ASSISTANCE

1) A Team put in place by the Organisers on the itinerary and at the Bivouac will be dedicated to checking all forms of forbidden assistance and will be authorised to apply the following penalties.

- 2) Assistance carried out by a non-accredited Vehicle or person will lead to the following penalties:
- 1st infringement: obligation for Vehicles and/or the persons concerned to be accredited with the Organiser or to leave the itinerary of the Rally + a 3-hour penalty for the concerned Competitor, 6 hours on a Selective Section;
 - Repeated offense: Disqualification of the assisted Competitor.
- 3) Carrying out assistance on a Selective Section (vehicle and/or person registered in the assistance category) will lead to penalties from 6 hours to Disqualification. All repeated offences will result in Disqualification.
- 4) Any presence of an assistance on a Road Section, at Start or Finish of a Selective Section when they do not feature on the assistance Road Book, will lead to penalties from 3 hours to Disqualification. All repeated offences will result in Disqualification.
- 5) Any transport of parts or assistance by a Vehicle not accredited, the dropping or parachuting of spare parts will lead to penalties from 6 hours to Disqualification. All repeated offences will result in Disqualification.
- 6) Carrying out assistance outside the Bivouac or in an enclosed place (inside or outside the Bivouac), will lead to penalties from 6 hours to Disqualification. A tent is not considered as an enclosed place. All repeated offences will result in Disqualification.
- 7) Signposting or the transmission of information by any means, by any means will be penalised by a 3-hour penalty + a €500 fine per infringement, for the concerned Competitor.
- 8) The presence of any means of transport (car, bike, aircraft, helicopter, etc.) following the Rally route the same day or a number of days prior, as well as the prolonged flying over of any means of transport, with official means or not, carry aboard a person having any link whatsoever with Competitors still in the race will lead to the immediate Disqualification of all the participants having a link with one of these means.
- 9) All airborne assistance not controlled by the Organisers will lead to penalties from 6 hours to Disqualification. All repeated offences will result in Disqualification.
Airborne assistance is considered as any presence on a Stage of an aircraft having aboard any person with any link whatsoever with a Competitor and any transport of assistance materials (tools, generators, lights, compressors, etc.) in private planes or planes chartered by the Organisers.

ART 12 - INSURANCE

See article 33P of the Specific Regulations + Appendix 2.

It is essential for you to read this article (33P1, 33P2, 33P3) so as to know the cover and guarantees included.

ART 13 - ADVERTISING

- 1) The Organisers shall supply each Crew with a set of identification plates comprising: 2 "Rally" plates and 3 panels bearing the race numbers which are called number plates.
- 2) Throughout the duration of the Event, the plates must be affixed according to the present Regulations.
In no case may they cover, even partially, the Vehicle's licence plates.

3) The number plates (dimensions still being studying on) must be affixed to the right and left sides of the Vehicle, on the area situated between the wheel arches, provided that they are totally visible from the side, as well as on the roof of the Vehicle, legible from the back.

They bear the race number, the name of the Event and possibly the name of the Organisers' main sponsor. 26 cm x 50 cm are for compulsory Organiser's advertising.

4) 2 "Rally" plates must be positioned legibly in a visible position during the whole Event. They must be fixed at the front and the rear of the Vehicle, parallel the axis of the wheels, without covering, even partially, the Vehicle's license plates and, except for the manufacturer's acronym, the plate must be the first inscription legible from the front above the line of the headlamps.

They incorporate the Competitor's race number in figures of 4 cm high and with a stroke thickness of 1 cm.

5) At any time during the Event, the absence or faulty positioning of a plate may incur, on certification, a cash penalty equivalent to 10% of the entry fees (Vehicle and Crew fee), on condition that the Organisers provide these plates. The absence or faulty positioning of 2 or more plates at the same time may result in a cash penalty equivalent to 20% of the entry fee (Vehicle and Crew fee).

6) The names of the first Driver and his co-driver(s), plus their national flags (nationality of the licence), of a height of 30 - 50 mm, must appear on both sides of the front wings or front doors of the Vehicle. Any Vehicle failing to comply with this rule may be subject to a cash penalty, on certification, equivalent to 10% of the entry fee (Vehicle and Crew fee).

7) An identity bracelet equipped with a RFID chip, on which will be printed **the phone number of the PCO (+33.1.41.33.15.81)**, will allow the identification of Crews. Crews and Competitors bracelets will be differentiated. Failure to wear this bracelet, noticed by an Official, will result in a penalty of 10% of the entry fee (individual fee). In the case where the bracelet becomes damaged the person must request a replacement from the Competitors' Department, in exchange for the damaged item.

8) Crews are allowed to affix any kind of advertising to their Vehicles, provided that:

a) it is authorised by the FFSA and the FIA Regulations and the legislation of the countries crossed,

b) it is not likely to give offence,

c) it does not encroach upon the spaces defined below reserved for Rally plates, number plates and windscreen strips,

d) it does not interfere with the crew's vision through the windows,

e) it does not express either a religious and/or political opinion.

9) The places reserved for the Organisers for collective advertising which may not be bought are situated on:

f) the number plates where the Organisers' advertising must be affixed on a strip measuring 26 cm x 50 cm, which may be divided above and/or below the numbers,

g) one set of "Rally" plates, of which 9 cm x 43 cm is reserved for the Organisers' advertising,

h) two strips to be affixed on each side of the upper part of the windscreen, 10 cm high by 25 cm long.

10) The Organisers' optional advertising will appear on two 50 cm wide x 52 cm high plates which cannot be subdivided, to be affixed on the right and left sides of the Vehicle, on the area situated between the wheel arches provided that they are totally visible from the side.

11) The Crews must ensure that the advertising is properly affixed throughout the entire Event from the moment they leave scrutineering. If compulsory or optional advertising is absent or wrongly fixed, a penalty of 10% of the entry fee (Vehicle and Crew fee) will be incurred for the first offence, and 100% of the entry fee for each repeated offence.

12) It is the Competitor's responsibility to ensure the adequate mounts, in order to conform with the here above mentioned rules; any kind of modification of the sticker is prohibited (cut, etc.).

ART 14 – SCRUTINEERING AND ADMINISTRATIVE CHECKS

The act of presenting a Vehicle for administrative checks and scrutineering is considered as an implicit declaration of its compliance.

The start will be refused to all Crews which present themselves at administrative checks and/or scrutineering after the time limits laid down in the Regulations, except in the case of force majeure, duly recognised as such by the Sporting Stewards.

14.1 SCRUTINEERING

1) Assistance vehicles leaving from Europe will pass scrutineering in Le Havre on 22nd or 23rd November 2012.

The presence of members of the Crew is not compulsory, vehicles may be passed through scrutineering by a representative of the team.

Vehicles presented directly in Lima will pass scrutineering at the Assistance Park, on 2nd, 3rd and 4th January 2013, at the end of administrative checks.

2) Crews will receive a convocation stating the day and exact time at which they must present their vehicle.

Failure to respect the convocation times will incur a penalty of:

- €50 for the 1st hour late (inclusive),
- €80 for the following hours (inclusive).

3) Crews undertake to present a vehicle conforming to the entry form, on pain of being refused the embarkation.

4) Scrutineering will consist of:

- checks on the conformity of the vehicle and safety equipment;
- the placing of stickers conforming with Article 16P of the present Regulations ;
- checks on the functioning of the Tripy connections.

5) Vehicles which leave from Europe on the ferry set up by the Organiser must be presented at Le Havre for embarkation with the Tripy mounts, cables and antenna in place ready to receive the system, on pain of being refused embarkation.

Vehicles presented directly in Lima, must be presented at scrutineering with the Tripy mounts, cables and antenna in place ready to receive the system. Failure to comply will result in a penalty of €150.

6) Vehicles must be presented at scrutineering ready to start and safety equipment listed at Article 15.1 of the Assistance Regulations should be presented to the Stewards in charge of the controls.

7) All vehicles which appear not to conform with given information, or are not adapted to Rally norms, during scrutineering may be refused embarkation or start (on decision of the Organising Committee).

In the last case, the entry fees will remain the property of the Organisers.

14.2 ADMINISTRATIVE CHECKS

1) The administrative checks will take place in Lima on 2nd, 3rd and 4th January 2013.

2) The presence of all Crew members is compulsory at administrative checks.

The person responsible for a vehicle undertakes to present valid papers. If one of the necessary documents is missing the vehicle will not be accepted.

At administrative checks each Crew must present the following original valid documents.

No photocopies or declarations of loss will be accepted.

The Crews of vehicles presented directly in Lima must pass administrative checks before scrutineering, otherwise the stickers serving to identify the vehicle will not be issued and the checks will not be validated.

3) Crews will receive a convocation stating the day and exact time at which they must present their vehicle.

Failure to respect the convocation times will incur a penalty of:

- €50 for the 1st hour late (inclusive),
- €80 for the following hours (inclusive).

4) Documents to be presented by Drivers and co-drivers:

- national driving licence: for trucks in assistance category, it is mandatory to only have one heavy weight licence; for the other categories, the document must be adapted to the registered vehicle;
- valid passport (valid until 30/06/2013);
- visas;
- vaccinations: no vaccination is compulsory. Recommended: diphtheria, tetanus, polio, meningitis (A & C), viral hepatitis A & B, tetanus, polio, typhoid, etc.

5) Documents to be presented for Vehicles:

- valid log book or registration certificate (provisional registrations not acceptable);
- insurance certificate (green card);
- permission of the owner to use the vehicle when they are not part of the Crew;
- complete Packing List of the vehicle.

ART 15 - CREWS' SAFETY

15.1 SURVIVAL EQUIPMENT

1) For safety reasons Crews must carry with them in their Vehicles the following material:

- 1 torch,
- 1 general map of South America (Berlitz type : Peru, Chile, Argentina) or the Road Safety map (given at the administrative checks),
- 1 first aid kit,
- 1 seat belt cutter per person,
- 1 safety triangle,
- 2 yellow fluorescent jackets per Vehicle,
- 1 Tripy (included in the entry fee).

2) All Crews unable to present all the above equipment at the start of a Stage will be refused a start until compliance has been achieved. On the spot checks will be carried out.

15.2 WITHDRAWAL

1) In the case of withdrawal, it is imperative that Crews inform, by all possible means, and as quickly as possible, the Dakar Race Control on +33 (0)1.41.33.15.81.

Failure to respect this important safety clause will result in the refusal of all future applications to take part in any Cross-Country Rally organised by A.S.O.

The Crew will also have to conform itself to Article 9.3.3 of the Assistance Regulations.

2) In addition, failure to respect the obligation to inform the Organisers in the case of withdrawal, where search operations are carried out, will result in the Crew in question being liable for the financial costs of search operations.

3) Crew that have withdrawn may in no circumstances be transported by the Organisers for part or all of the route.

15.3 SAFETY BELT - HARNESS

1) The wearing of a safety belt or harness is compulsory over the entire itinerary.

2) Failure to wear the safety belt or harness will incur the following penalties:

- 1st infringement: fines of €300 per infringement, per Crew member;



- 2nd infringement: sealing of the vehicle until the TC at the End of Stage of the next Bivouac,
- 3rd infraction: Disqualification.

Checks will be made on the itinerary by the Organisers' safety and prevention team and will be forwarded to the Assistant Clerk of the Race responsible for Service Vehicles.

ART 16 – ACCESSORIES AND NAVIGATION

16.1 SATELLITE TELEPHONES

Carrying a satellite telephone is optional, but highly recommended in order to be able to communicate with the racing Competitors in the case of an accident or of a breakdown. The Organisers cannot be held responsible for the transmission of information between a racing Vehicle and its assistance.

16.2 RADIOS

1) For safety reasons assistance vehicles will be allowed to be fitted with VHF radio receivers/transmitters homologated by the Organisers and supplied by their suppliers only. They will use a sole frequency, all modifications are forbidden. The conditions concerning the receiving and installation of the equipment will be sent by the relevant supplier.

2) All UHF - CB transmitters and/or receivers and all other means of communication are forbidden throughout the entire Rally itinerary aboard vehicles.

3) AM/FM radio receivers must be commercially available models and not modified. The wave length of FM receivers is limited to 88 – 108 Mhz. Spot checks will be made. All infractions will result in penalties up to and including Disqualification.

4) Only Walkie-Talkies operating on one single frequency, used within the confines of the Bivouac are admissible. If they are not supplied by the Organisers' radio supplier (TDCOM), authorisation must be demanded from the authorities of the countries crossed and the frequencies used must be given to the Organisers.

16.3 SATELLITE LINKS

All satellite links or other links between a race Vehicle and an exterior base or another vehicle, is forbidden.

The use of a BGAN or RBGAN station (+ associate antenna) will be authorised exclusively in the Bivouac.

16.4 NAVIGATION

1) The use of an additional GPS not provided by the Organisers' supplier is authorised.

2) To ensure the respect of the official itinerary of the Road Book and to check speeds, the installation of the Tripy in a place visible by the Driver is strongly recommended.

ART 17 - PENALTIES

TABLE OF PENALTIES	Art No.	Financial penalties	Sealing of Vehicle until finish TC of following Stage	Start refused / Disqualification	Other penalties
Parking of a not accredited camping-car or mobile-home within a 2-km radius from the Bivouacs -1st offence -repeated offence	1.1.b	€ 700		up to X	
Change of Crew -per infraction and per Crew member	2.4	€ 500			
Absence at Briefing of at least one Crew member	3	€ 500			
Start outside schedule	6.2	€ 500			
Tripy: all activity or fraud by Crew: -1 st offence: -2 nd offence: -3 rd offence: -4 th offence:	7.2.4	€ 300 € 500	X	X	
Non-respect of itinerary -1 st offence: -2 nd offence: -3 rd offence: -4 th offence:	8.2.2	€ 300 € 500	X	X	
Incivility -1 st offence: -repeated offence: to be decided by Assistant Clerk of Race - Service Vehicles	9.1.2	€ 500		up to X	
Leaving wheel or tyre -per wheel or tyre -repeated offence: to be decided by Assistant Clerk of Race - Service Vehicles	9.1.3	€ 1,000		up to X	
Involved in accident -according to circumstances, after inquiry	9.2			up to X	
Leaving , selling or giving parts transported, in addition to local penalties -per material -repeated offence:	9.3.3	€ 500		up to X	

TABLE OF PENALTIES	Art No.	Financial penalties	Sealing of Vehicle until finish TC of following Stage	Start refused / Disqualification	Other penalties
Leaving, selling or giving parts transported, in addition to local penalties	9.3.4				Refusal of inscription for 5 years
Damaged Vehicle (totally or partially destroyed), infraction of Article 9.3.6	9.3.6	€ 1,500			
Passing border with forbidden products, in addition to local penalties, from ... up to ...	9.3.7	€ 500		up to X	
Non-respect of Highway Code, excessive speed, dangerous driving	10			up to X	
Speeding from 1 to 20 kph in Speed Control Zones -for a cumulative distance less than 500 m -for a cumulative distance between 500 and 1,000 m -for a cumulative distance over 1,000 m	10.1.3	€ 200 € 400		X	
Maximum Speed allowed Speeding from 1 to 20 kph -for a cumulative distance less than 1,000 m -for a cumulative distance between 1,000 and 2,000 m -for a cumulative distance over 2,000 m Speeding from 21 to 40 kph -for a cumulative distance less than 1,000 m -for a cumulative distance between 1,000 and 2,000 m -for a cumulative distance over 2,000 m Speeding over 41 kph -for a cumulative distance less than 500 m -for a cumulative distance over 500 m	10.2.2	€ 200 € 400 € 600 € 400 € 600 € 800 € 800 € 1,000	X X		
Speeding, dangerous driving in Bivouac or Stage town	10.3			up to X	

TABLE OF PENALTIES	Art No.	Financial penalties	Sealing of Vehicle until finish TC of following Stage	Start refused / Disqualification	Other penalties
Assistance in Selective Section before closing of finish control -for assisted Competitor	11.1.2			X	
Refuelling on a Selective Section before closing of finish control -for assisted Competitor	11.1.6			X	
Assistance by non-accredited person or Vehicle -1 st offence -for assisted Competitor: -repeated offence -for assisted Competitor:	11.2.2			X	3h or 6h
Assistance on a Selective Section -for assisted Competitor: 6h to Disqualification -repeated offence -for assisted Competitor:	11.2.3			up to X X	6h
Presence of assistance in forbidden place -for assisted Competitor: 3h to Disqualification -repeated offence -for assisted Competitor:	11.2.4			up to X X	3h
Transport of parts by non-accredited Vehicle; dropping of material -for assisted Competitor: 6h to Disqualification -repeated offence -for assisted Competitor:	11.2.5			up to X X	6h
Assistance outside Bivouac or in closed place (in or outside Bivouac) -for assisted Competitor: 6h to Disqualification -repeated offence -for assisted Competitor:	11.2.6			up to X X	6h
Signposting or transmission of info -per offence for the assisted Competitor	11.2.7	€ 500			3h
Presence of means of transport -for assisted Competitor/s:	11.2.8			X	

TABLE OF PENALTIES	Art No.	Financial penalties	Sealing of Vehicle until finish TC of following Stage	Start refused / Disqualification	Other penalties
Airborne assistance not managed by organisation -for assisted Competitor: 6h to Disqualification -repeated offence -for assisted Competitor:	11.2.9			up to X X	6h
Delay for scrutineering and administrative checks -1 st started delayed hour -each next hour -over decided hour limits	14.1.2 & 14.2.3	€ 50 € 80		X	
Missing mounting brackets, cables and Tripy aerals: -in Le Havre -in Lima	14.1.5	€ 150			refused embarkation
Missing or incomplete safety material at a Stage start, until compliance with rules:	15.1.2			X	
PCO not informed of a Team withdrawal, for the concerned Team:	15.2.1				Refusal of following inscription + financial responsibility in case of research
Missing seat belt of harness, per infraction and per Crew member: -1 st infringement: -2 nd infringement: -3 rd infringement:	15.3	€ 300	X	X	
Non-complying radio transmitters and/or receivers:	16.2.3			up to X	

2. TECHNICAL REGULATIONS

1) TECHNICAL OBLIGATIONS

Each assistance vehicle must have the following equipment present:

- 1 homologated 2 kg manual powder fire extinguisher;
- External rear view mirrors;
- 2 towing points (1 at the front, 1 at the back);
- 1 towing strap;
- reinforced windscreen;
- fuel tanks. The Crew will be responsible for calculating its autonomy;
- 1 air horn;
- 2 spare wheels.

In addition for trucks:

- Individual seats for all Crew members (bench seats forbidden).

2) HEIGHT

Maximum height of cars (including roof rack) 2.00 m.

Maximum height of trucks (including roof rack) 4.00 m.

3) ROLL-CAGES

For cars and trucks, roll-cages are not compulsory.

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